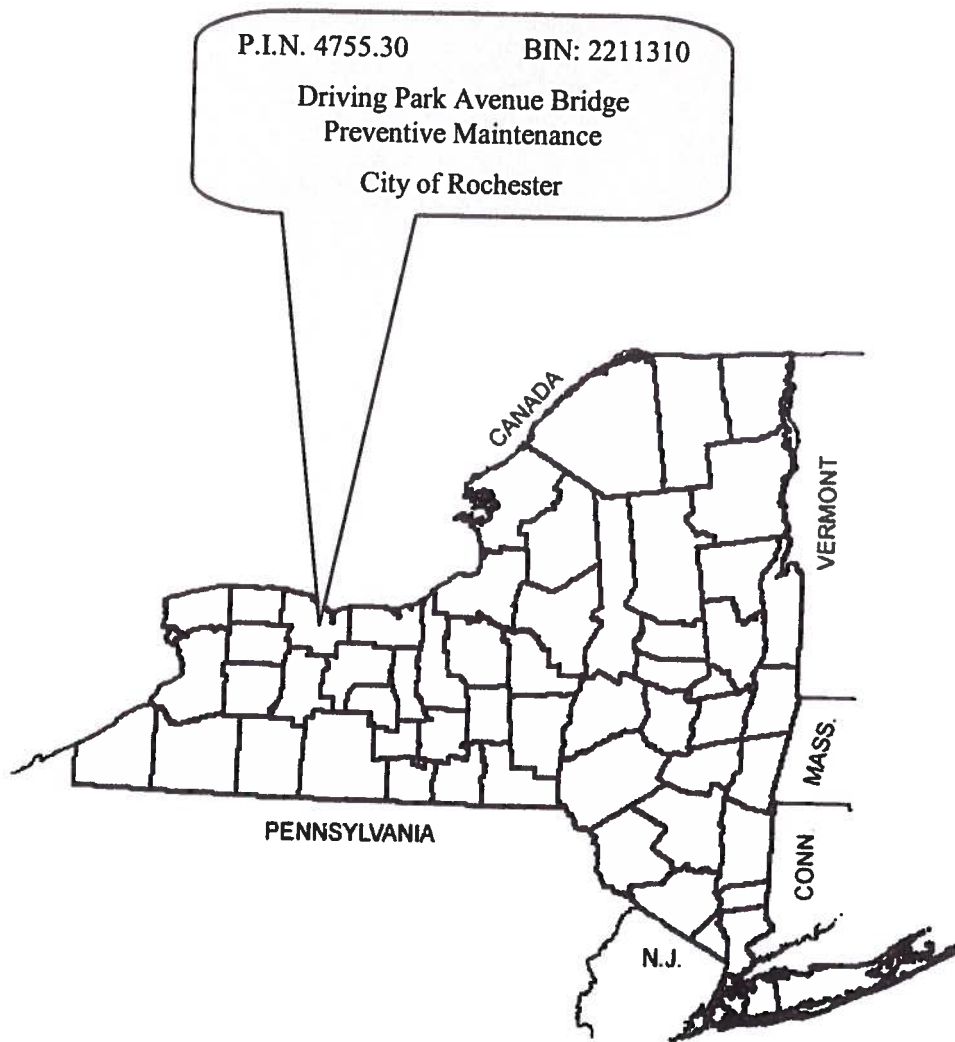


# TRANSPORTATION

## INITIAL PROJECT PROPOSAL/ FINAL DESIGN REPORT

March 2012



PROJECT REPORT

U.S. Department of Transportation Federal Highway Administration

NEW YORK STATE DEPARTMENT OF TRANSPORTATION  
ANDREW M. CUOMO, Governor      JOAN McDONALD, Commissioner



# PROJECT APPROVAL SHEET

(Pursuant to SAFETEA-LU Matrix)

## Milestones

## Signatures

## Dates

### A. IPP Approval:

The project cost and schedule are consistent with the Regional Capital Program. The IPP was signed by:

See Appendix F for IPP signature

\_\_\_\_\_  
Regional Director, NYSDOT Region 4

### B. Recommendation for Scoping & Design Approval:

The project cost and schedule are consistent with the Regional Capital Program.

### Environmental Determination & Federal Aid Process Concurrence:

The NYSDOT on behalf of FHWA (based on the NEPA Checklist) concurs with the classification of this project as a NEPA Class II, Programmatic Categorical Exclusion as described in this document.

  
\_\_\_\_\_  
Dan Hallowell

NYSDOT R4, Regional Planning & Program Manager

4/19/12

### C. Recommendation for Scope, Design & Nonstandard Feature Approval:

Procedurally, this project was progressed using the NYSDOT Locally Administered Federal Aid Procedures Manual. All requirements requisite to these actions and approvals have been met, the required independent quality control reviews separate from the functional group reviews have been accomplished, and the work is consistent with established standards, policies, regulations and procedures, except as otherwise noted and explained.

  
\_\_\_\_\_  
John M. Papponetti, P.E.

Associate | Project Manager  
LaBella Associates, P.C.

3/8/12

### D. Public Hearing Certification (23 USC 128):

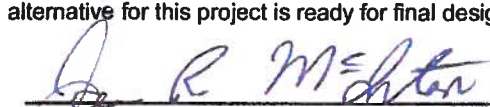
A public hearing was not required. A public information meeting was not conducted.

### Nonstandard Feature Approval:

No nonstandard features have been identified, created, or retained.

### Scoping & Design Approval:

The required environmental determinations have been made and the preferred alternative for this project is ready for final design.

  
\_\_\_\_\_  
James McIntosh, P.E.

City Engineer  
Department of Environmental Services

3/12/12

Driving Park Ave. Bridge  
4755.30

# LIST OF PREPARERS

## Group Director Responsible for Production of the Design Approval Document:

**John M. Papponetti, P.E., Associate | Project Manager, LaBella Associates, P.C.**  
Description of Work Performed by Firm: Directed the preparation of the Design Approval Document in accordance with established standards, policies, regulations and procedures, except as otherwise explained in this document.



**Note:** It is a violation of law for any person, unless they are acting under the direction of a licensed professional engineer, architect, landscape architect, or land surveyor, to alter an item in any way. If an item bearing the stamp of a licensed professional is altered, the altering engineer, architect, landscape architect, or land surveyor shall stamp the document and include the notation "altered by" followed by their signature, the date of such alteration, and a specific description of the alteration.

**PIN:** 4755.30

**PROJECT NAME:** Driving Park Avenue Bridge Preventive Maintenance

**MUNICIPALITY:** City of Rochester

**COUNTY:** Monroe

**ROUTE/SH #:** NA

**BIN:** 2211310

**LIMITS:** Milepoints: NA

Reference Markers: NA

**PROJECT LENGTH:** NA

**FEDERAL AID SYSTEM:** Non-NHS

**FUNCTIONAL CLASS:** Urban-Minor Arterial

**EXISTING AADT:** 19,955 (from 2004 count)

**TRUCKS (%):** 5%

**EXISTING CHARACTERISTICS OF CONCERN:** The existing Span 3 bridge deck currently exhibits severe, chronic, and full depth transverse cracking over the transverse floorbeams of the steel arch superstructure span. The End Abutment and Spans 2 & 3 expansion joints are not functioning as intended and exhibit active leakage. The deck slab cracks and failed expansion joints are allowing salt-laden runoff to penetrate the deck, resulting in the corrosion of the steel superstructure below. The weathering steel girders, floorbeams, and diaphragms exhibit minor delaminations and scaling beneath the expansion joints and cracks in the deck. Bearings at Pier 2 & 3 exhibit moderate corrosion. Scupper downspouts have separated in Span 4. The bedding mortar beneath curb is crumbling or missing at sporadic locations in all spans. The existing bridge curb is slightly separated from the sidewalk in all spans.

**ELEMENT**

BIN 2211310

**MEASURE/INDICATOR**

Condition Rating is 4.972; Sufficiency Rating is 94.5

- Joints (End Abutment): 4
- Joints (Span 3 & 4): 3
- Deck (Span 3): 4
- Curbs (All Spans): 4
- Scuppers (Spans 3 & 4): 2
- Paint (Spans 2 – 4): 4
- Bearings (Span 3 & 4): 4

Refer to Appendix D for copies of the 2010 NYSDOT Biennial Inspection Report.

**PROJECT OBJECTIVE(S):** This project will complete element specific bridge repairs to keep the City's critical infrastructure in good working order. The major element specific repair will include the complete replacement of the Span 3 structural deck. The proposed repairs will extend the functional life of the structure approximately 20-25 years.



## PROJECT ELEMENT(S) TO BE ADDRESSED:

- |   |  |
|---|--|
| <input type="checkbox"/> Highway Element-Specific           | <input type="checkbox"/> Operational Maintenance |
| <input checked="" type="checkbox"/> Bridge Element-Specific | <input type="checkbox"/> Where & When            |
| <input type="checkbox"/> Other:                             |  |

**DESCRIPTION OF PROPOSED WORK:** The work to be undertaken through this project is as follows:

- Remove & Replace Structural Deck (Span 3)
- Remove & Replace Sidewalks (Span 3)
- Remove/Store & Reset Bridge Railing & Fencing (Span 3)
- Structural Steel Repairs (Span 3)
- Replace Bridge Curb (Span 3)
- Remove and Reset Light Poles (Span 3)
- Replace Scuppers (Span 3)
- Remove & Reset Bridge Curb
- Replace Expansion Joints
- Paint Girder Ends & Floorbeams at Joint Locations
- Clean and Paint Bearings under Joints
- Wash Bridge & Flush Scuppers

### Clean and Paint Bearings under Joints Clarification:

Per the latest biennial inspection report (2010) the "Span 2 end expansion bearings at Pier 2 exhibit moderate corrosion at G1, G2, G7 & G8. Conditions are worst at G2. However, the stainless steel sliding surfaces appear fully functional and the bearing position is normal. The Span 3 expansion bearings at Pier 2 exhibit minor corrosion and would rate 5. The arch thrust bearings would rate 6. Monitoring for "wear" on the 12.5" diameter bearing pins has shown no change since 1998. Detailed monitoring of these pins is not warranted and documentation is discontinued in this inspection."

"The Span 4 begin fixed bearing at Pier 3 exhibit moderate corrosion at G1, G2, G7 & G8. Conditions are worst at G2, where debris is piled against the bearing. The fixed bearings appear fully functional and there is no distress. The Span 3 end expansion bearings at Pier 3 exhibit minor corrosion and would rate 5. The arch thrust bearings would rate 6. Monitoring for "wear" on the 12.5" diameter bearing pins has shown no change since 1998. Detailed monitoring of these pins is not warranted and documentation is discontinued in this inspection."

Based on this information it is anticipated that field cleaning and painting the bearing assembly to protect from future corrosion is all that is warranted at this time.

### Scupper Replacement Clarification:

Scuppers in Span 3 will be replaced due to the removal and replacement of the existing deck. The existing scupper downspouts will be left in place. New scuppers will be piped to the existing downspout system.

### Curb Resetting and Replacement Clarification:

Several pieces of curb need replacing or resetting each year at this site. A nominal amount of curb replacement and resetting has been accounted for in Spans 1, 2 & 4.

Bridge Washing Clarification:

The Genesee River is a Class B stream at the Driving Park bridge location. A Class B stream is best used for swimming and other contact recreation, but not for drinking water. The City performs bridge washing operations on all City owned bridges over the Genesee River on a yearly basis. This effort has been coordinated with NYSDEC in the past and as long as the Environmental Waterway Protection specification is enforced, the only requirement received from DEC is that bridge washing should be performed during times of high flow.

Structural Steel Repair Clarification:

Based on the condition of Span 3 concrete deck and joints (paint rating of 2 for weathering steel), minor steel repairs will be necessary. Limits of steel repairs required will be determined during preliminary design.

**PRIORITY RESULTS:**    ☒ Mobility & Reliability    ☐ Safety    ☐ Security  
                                 ☐ Economic Competitiveness    ☐ Environmental Stewardship

**FUNDING SOURCE:**    ☐ 100% State    ☒ Federal

**SEQRA AND NEPA CLASSIFICATION:**

SEQRA Type:    ☐ Exempt    ☒ Type II

NEPA Class:    ☒ Class II - Automatic CE  
                         ☐ Class II - Programmatic CE  
                         ☐ N/A – Project is 100% State funded

The following Checklist(s) are attached in Appendix E:

- ☒ NEPA Checklist
- ☒ Environmental Checklist
- ☒ Section 106 Project Submittal Package

**ENVIRONMENTAL DOCUMENTATION:**

**Archeological Resources** – The project is located within an Archeological Sensitive Area. However, the project will have no effect on these resources due to the fact that the project only consists of element specific repairs to previously disturbed areas. A Section 106 Project Submittal Package was sent to the NYSDOT Region 4 Cultural Resource Officer for a determination of effect. NYSDOT has determined that the project activities have no potential to cause effects on historic properties in accordance with 36 CFR 800.3(a)(1) therefore, there are no further obligations for compliance with Section 106 of the National Historic Preservation Act.

**Floodplains** – The project is located within a FEMA designated 100-yr floodplain. The project will have no effect on the existing floodplain since no modifications are being undertaken to the existing hydraulic characteristics of the bridge.

**Permitting** – Typically for element specific repair projects that does not involve in-stream work, NYSDEC and USACOE has no jurisdiction. Since this project will include removal of the existing Span 3 deck and underside steel repairs and painting, NYSDEC will require the implementation of a waterway protection system. This is covered through the use of NYSDOT's Standard Specification for Environmental Waterway Protection and specific notes placed on the contract plans indicating that the contractor is not allowed to drop any debris to the area below the bridge. A letter seeking

concurrence to this assumption will be sent during the preliminary design phase of the project.

See section "Description of Proposed Work" – Bridge Washing Clarification for additional NYSDEC coordination requirements.

**Endangered or Threatened Species** – The Bog Turtle is a threatened species known to be found in the Town of Riga (outside of project limits). The American Burying Beetle is an endangered animal known to be found in the Rochester area at one time. Based on available NYSDEC documents, the American Beetle is known to exist in only two locations, Block Island, RI and Eastern Oklahoma. NYSDEC also indicates that the Purple Bluets is known to exist along the river banks of the Genesee River. This project does not propose any activities that would impose a negative impact on endangered or threatened species.

Refer to Appendix E for supplemental documentation for the above environmental concerns.

#### **DESIGN STANDARDS:**

Guidance on establishing standards for this Bridge Preventive Maintenance Project will be obtained from the NYSDOT Bridge Manual and Highway Design Manual.

Table A Critical Design Elements for Driving Park Avenue Bridge					
PIN:		4755.30	NHS (Y/N):		No
Route No. & Name:		Driving Park Avenue Bridge	Functional Classification:		Minor Arterial
Project Type:		Preventive Maintenance	Design Classification:		Urban-Minor Arterial (HDM Exhibit 2-1)
% Trucks:		5	Terrain:		Level
ADT:		19,955	Truck Access/Qualifying Hwy.		Neither
Element		Standard		Existing Condition	Proposed Condition
1	Design Speed <sup>1</sup>	30 mph HDM Section 2.7.4.1.A		30 mph (Posted)	30 mph
2	Lane Width	9 ft minimum Bridge Manual (BM) Section 2.3.1 Table 2-1 and App. 2A. Tables N & X or HDM Section 2.7.4.1.B, Exhibit 2-7		3 lanes @ 12 ft	3 lanes @ 12 ft
3	Shoulder Width	2 ft minimum, BM Section 2.3.1 Table 2-1, and App. 2A Tables N & X or HDM Section 2.7.4.1.C, Exhibit 2-7		4 ft	4 ft
4	Bridge Roadway Width	2(9) + 2(2) = 22 ft Min. BM Section 2.3.1 Table 2-1 and App. 2A Tables N & X		44 ft	44 ft
5	Maximum Grade	7% HDM Section 2.7.4.1.E, Exhibit 2-7		2% max.	2% max.
6	Horizontal Curvature	231 ft (e = 6.0%) HDM Section 2.7.4.1.F, Exhibit 2-7		NA	NA
7	Superelevation	6% Maximum HDM Section 2.7.4.1.G		NA	NA
8	Stopping Sight Distance	200 ft Minimum HDM Section 2.7.4.1.H, Exhibit 2-7		200 ft min.	200 ft min.
9	Horizontal Clearance	6 ft without barrier; with barrier use greater of shoulder width or 4 ft, except on bridges where the NYSDOT BM Section 2 allows less HDM Section 2.7.4.1.I		4 ft	4 ft
10	Vertical Clearance	14 ft Minimum, Highway 14'-6" Desirable, Highway 16'-6" Minimum for Thru-Truss BM Section 2.4.1 Table 2-2		NA	NA
11	Pavement Cross Slope	1.5% Min. to 2% Max. HDM Section 2.7.4.1.K		2%	2%
12	Rollover	4% between lanes; 8% at EOT; HDM Section 2.7.4.1.L		4%	4%
13	Structural Capacity	Bridge Rehabilitation: HS 20 Live Load HDM Section 2.7.4.1.M & BM Section 2.6.1		HS 20	HS 20
14	Level of Service	NA		NA	NA
15	Control of Access	NA		NA	NA
16	Pedestrian Accommodations	Complies with HDM Chapter 18		NA	NA
17	Median Width	NA		NA	NA
(1) The design speed of 30 mph was chosen based upon the posted speed limit, type of terrain, volume, and road classification.					
(2) **Denotes non-standard feature.					

**Non-Standard/Non-Conforming Features** – There are no nonstandard or nonconforming features within the project limits.

#### PLANS:

See Appendix B for applicable plans, elevations, and sections.

**MPO INVOLVEMENT:** ☐ No ☒ Yes  
TIP Name: Driving Park Avenue Bridge Preventive Maintenance  
TIP No.: B11-19-MN1

**TIP AMENDMENT REQUIRED:** ☒ No ☐ Yes Needed by:

**STIP STATUS:**☒ On STIP☐ Not on STIP**NOTES ON SPECIAL CIRCUMSTANCES:** NA**SPECIAL TECHNICAL ACTIVITIES REQUIRED:** NA**PLANNED PUBLIC INVOLVEMENT:**

The nature of the project is Element Specific Bridge Maintenance therefore; input from residents during preliminary and final design is not being solicited. Coordination with Utility companies within the project area will be completed in final design as needed. During construction, press releases and other media alerts will be used to increase public awareness. Motorist information strategies will include daily updates to traffic through the radio, and temporary motorist information signs.

**WORKZONE SAFETY & MOBILITY:**

The Region has determined that the subject project is not significant per 23 CFR 630.1010. A Transportation Management Plan (TMP) consisting of a temporary work zone traffic control plan will be prepared during final design. Coordination with the Regional Transportation Operations Center and public information activities will be considered during final design.

**PROBABLE SCHEDULE AND COST:****DESIRED LETTING:** January 2014**SCHEDULE ISSUES:**☐

Public Meeting

☐

4(f)/106 FHWA sign-off

☐

Permits

☐

Other - Identify

☐

Consultant(s) for:

☐

No Consultant Needed

Project Phase	Activity Duration	Estimated Cost	Fund Source	Obligation Date
Design	12 months	\$261,000	HBP (80%) Local (20%)	FFY 2011
Construction	12 months	\$2,563,000	HBP (80%) Local (20%)	NA
Construction Inspection	12 months	\$256,000	HBP (80%) Local (20%)	NA
<b>TOTAL</b>		<b>\$3,080,000</b>		

**BASIS OF ESTIMATE:** Design Phase Estimates are based on the Consultant's past experience with similar types of projects. Construction Estimate is based on past bid results for similar construction tasks. See Appendix C for Preliminary Estimate.

**PROGRAM DISPOSITION:** NA**PROJECT CATEGORY:**☒

Maintenance

**STATEWIDE SIGNIFICANCE:**☒

No

**ASSET MANAGEMENT (OPTIONAL):**☐

Applies

☒

Not Applicable

**ROW:**

No ROW is required to complete the scope of work for this project. The ROW Clearance Certificate will be attached to the PS&E transmittal memo.

**PUBLIC FRIENDLY DESCRIPTION OF PROJECT:**

The project consists of completing element specific repairs to the Elmwood Avenue Bridge over Genesee River.

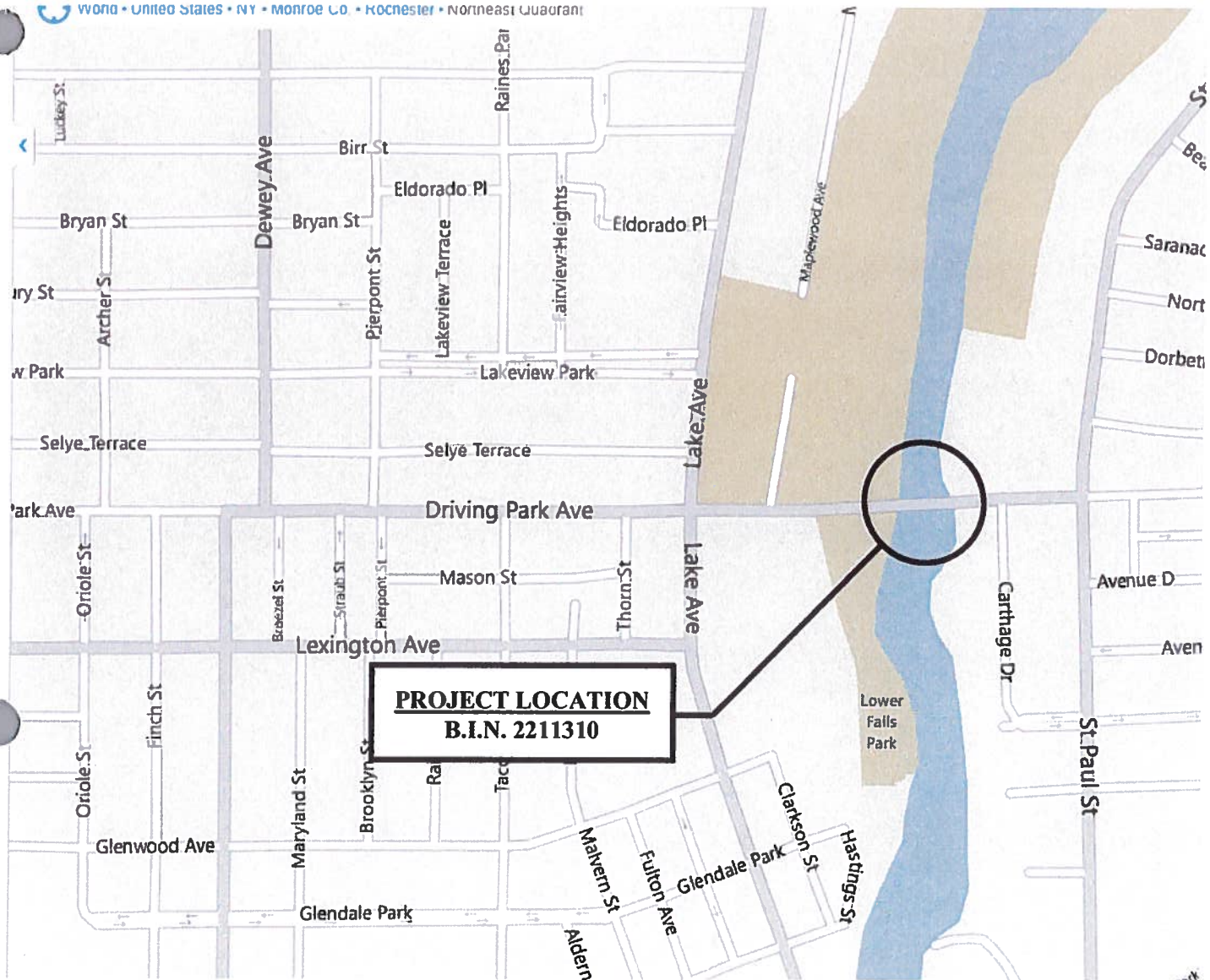
**PROJECT MANAGER/JOB MANAGER:** Thomas Hack, P.E.  
**FUNCTIONAL AREA(S):** City of Rochester Department of Environmental Services  
**PHONE(S):** 585-428-6852

**ORIGINAL IPP PREPARED BY:** Edwin Welsh      **DATE:** February 23, 2011  
NYSDOT – Region 4

# **APPENDIX A**

## **Location Maps**





## PROJECT LOCATION MAP

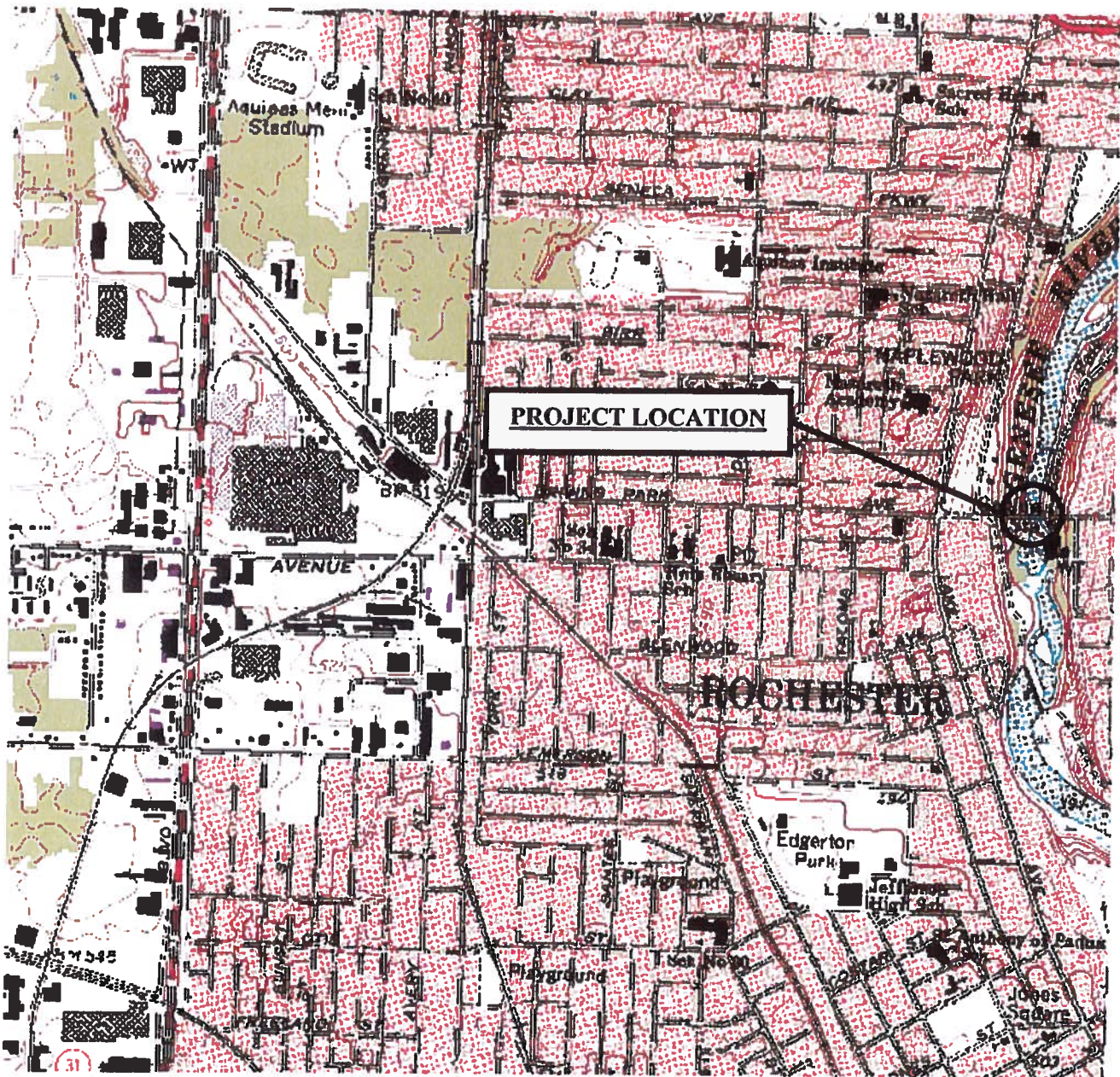
**Driving Park Avenue Bridge  
over Genesee River  
(BIN 2211310)**

**City of Rochester  
Monroe County, New York**

**LABELLA**  
Associates, P.C.

PROJECT NO.: 207650.04





## USGS LOCATION MAP

Driving Park Avenue over Genesee River  
(BIN 2211310)

USGS Quadrangle Map: Rochester West

City of Rochester  
Monroe County, New York

**ABELLA**  
Associates, P.C.

PROJECT NO.: 207650.04



# **APPENDIX B**

**Plan, Elevation & Sections**

1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100	101	102	103	104	105	106	107	108	109	110	111	112	113	114	115	116	117	118	119	120	121	122	123	124	125	126	127	128	129	130	131	132	133	134	135	136	137	138	139	140	141	142	143	144	145	146	147	148	149	150	151	152	153	154	155	156	157	158	159	160	161	162	163	164	165	166	167	168	169	170	171	172	173	174	175	176	177	178	179	180	181	182	183	184	185	186	187	188	189	190	191	192	193	194	195	196	197	198	199	200	201	202	203	204	205	206	207	208	209	210	211	212	213	214	215	216	217	218	219	220	221	222	223	224	225	226	227	228	229	230	231	232	233	234	235	236	237	238	239	240	241	242	243	244	245	246	247	248	249	250	251	252	253	254	255	256	257	258	259	260	261	262	263	264	265	266	267	268	269	270	271	272	273	274	275	276	277	278	279	280	281	282	283	284	285	286	287	288	289	290	291	292	293	294	295	296	297	298	299	300	301	302	303	304	305	306	307	308	309	310	311	312	313	314	315	316	317	318	319	320	321	322	323	324	325	326	327	328	329	330	331	332	333	334	335	336	337	338	339	340	341	342	343	344	345	346	347	348	349	350	351	352	353	354	355	356	357	358	359	360	361	362	363	364	365	366	367	368	369	370	371	372	373	374	375	376	377	378	379	380	381	382	383	384	385	386	387	388	389	390	391	392	393	394	395	396	397	398	399	400	401	402	403	404	405	406	407	408	409	410	411	412	413	414	415	416	417	418	419	420	421	422	423	424	425	426	427	428	429	430	431	432	433	434	435	436	437	438	439	440	441	442	443	444	445	446	447	448	449	450	451	452	453	454	455	456	457	458	459	460	461	462	463	464	465	466	467	468	469	470	471	472	473	474	475	476	477	478	479	480	481	482	483	484	485	486	487	488	489	490	491	492	493	494	495	496	497	498	499	500	501	502	503	504	505	506	507	508	509	510	511	512	513	514	515	516	517	518	519	520	521	522	523	524	525	526	527	528	529	530	531	532	533	534	535	536	537	538	539	540	541	542	543	544	545	546	547	548	549	550	551	552	553	554	555	556	557	558	559	560	561	562	563	564	565	566	567	568	569	570	571	572	573	574	575	576	577	578	579	580	581	582	583	584	585	586	587	588	589	590	591	592	593	594	595	596	597	598	599	600	601	602	603	604	605	606	607	608	609	610	611	612	613	614	615	616	617	618	619	620	621	622	623	624	625	626	627	628	629	630	631	632	633	634	635	636	637	638	639	640	641	642	643	644	645	646	647	648	649	650	651	652	653	654	655	656	657	658	659	660	661	662	663	664	665	666	667	668	669	670	671	672	673	674	675	676	677	678	679	680	681	682	683	684	685	686	687	688	689	690	691	692	693	694	695	696	697	698	699	700	701	702	703	704	705	706	707	708	709	710	711	712	713	714	715	716	717	718	719	720	721	722	723	724	725	726	727	728	729	730	731	732	733	734	735	736	737	738	739	740	741	742	743	744	745	746	747	748	749	750	751	752	753	754	755	756	757	758	759	760	761	762	763	764	765	766	767	768	769	770	771	772	773	774	775	776	777	778	779	780	781	782	783	784	785	786	787	788	789	790	791	792	793	794	795	796	797	798	799	800	801	802	803	804	805	806	807	808	809	810	811	812	813	814	815	816	817	818	819	820	821	822	823	824	825	826	827	828	829	830	831	832	833	834	835	836	837	838	839	840	841	842	843	844	845	846	847	848	849	850	851	852	853	854	855	856	857	858	859	860	861	862	863	864	865	866	867	868	869	870	871	872	873	874	875	876	877	878	879	880	881	882	883	884	885	886	887	888	889	890	891	892	893	894	895	896	897	898	899	900	901	902	903	904	905	906	907	908	909	910	911	912	913	914	915	916	917	918	919	920	921	922	923	924	925	926	927	928	929	930	931	932	933	934	935	936	937	938	939	940	941	942	943	944	945	946	947	948	949	950	951	952	953	954	955	956	957	958	959	960	961	962	963	964	965	966	967	968	969	970	971	972	973	974	975	976	977	978	979	980	981	982	983	984	985	986	987	988	989	990	991	992	993	994	995	996	997	998	999	1000	1001	1002	1003	1004	1005	1006	1007	1008	1009	1010	1011	1012	1013	1014	1015	1016	1017	1018	1019	1020	1021	1022	1023	1024	1025	1026	1027	1028	1029	1030	1031	1032	1033	1034	1035	1036	1037	1038	1039	1040	1041	1042	1043	1044	1045	1046	1047	1048	1049	1050	1051	1052	1053	1054	1055	1056	1057	1058	1059	1060	1061	1062	1063	1064	1065	1066	1067	1068	1069	1070	1071	1072	1073	1074	1075	1076	1077	1078	1079	1080	1081	1082	1083	1084	1085	1086	1087	1088	1089	1090	1091	1092	1093	1094	1095	1096	1097	1098	1099	1100	1101	1102	1103	1104	1105	1106	1107	1108	1109	1110	1111	1112	1113	1114	1115	1116	1117	1118	1119	1120	1121	1122	1123	1124	1125	1126	1127	1128	1129	1130	1131	1132	1133	1134	1135	1136	1137	1138	1139	1140	1141	1142	1143	1144	1145	1146	1147	1148	1149	1150	1151	1152	1153	1154	1155	1156	1157	1158	1159	1160	1161	1162	1163	1164	1165	1166	1167	1168	1169	1170	1171	1172	1173	1174	1175	1176	1177	1178	1179	1180	1181	1182	1183	1184	1185	1186	1187	1188	1189	1190	1191	1192	1193	1194	1195	1196	1197	1198	1199	1200	1201	1202	1203	1204	1205	1206	1207	1208	1209	1210	1211	1212	1213	1214	1215	1216	1217	1218	1219	1220	1221	12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# **APPENDIX C**

## **Construction Estimate Backup**

**DRIVING PARK AVENUE BRIDGE OVER GENESEE RIVER  
PREVENTIVE MAINTENANCE  
CITY OF ROCHESTER**

BIN 2211310

Engineer's Estimate of Probable Cost

LABELLA ASSOCIATES  
300 STATE STREET  
ROCHESTER, NY 14614

Date: 2/13/2012

ITEM NO.	ITEM DESCRIPTION	PAY UNIT	UNIT PRICE	EST. QUANTITY	TOTAL COST
557.0103	SUPERSTRUCTURE SLAB WITH INTEGRAL WEARING SURFACE - TYPE 3 FRICTION	SY	\$320.00	3200	\$1,024,000
557.30	SIDEWALKS AND SAFETY WALKS	SY	\$100.00	1000	\$100,000
558.02	LONGITUDINAL SAWCUT GROOVING OF STRUCTURAL SLAB SURFACE	SY	\$15.00	2200	\$33,000
559.16960118	PROTECTIVE SEALING OF STRUCTURAL CONCRETE	SF	\$1.00	8400	\$8,400
559.18960118	PROTECTIVE SEALING OF STRUCTURAL CONCRETE ON NEW BRIDGE DECKS & BRIDGE DECK OVERLAYS	SF	\$1.00	44500	\$44,500
564.100001	STRUCTURAL STEEL REPLACEMENT	LB	\$2.00	25000	\$50,000
567.60	ARMORLESS BRIDGE JOINT SYSTEM	FT	\$225.00	250	\$56,250
574.02	STRUCTURAL STEEL PAINTING: OVERCOATING	LS	\$50,000.00	1	\$50,000
576.02	SCUPPERS (TYPE B)	EA	\$1,000.00	10	\$10,000
587.02	BRIDGE RAILING REMOVAL AND STORAGE	FT	\$20.00	1000	\$20,000
587.03	INSTALLATION OF STORED BRIDGE RAILING	FT	\$45.00	1000	\$45,000
580.02	REMOVAL OF STEEL SUPPORTED STRUCTURAL SLAB (WITH SHEAR CONNECTORS)	SF	\$15.00	28200	\$423,000
609.0302	GRANITE BRIDGE CURB - TYPE F1	FT	\$40.00	900	\$36,000
615.09	RESETTING EXISTIN CURB	FT	\$35.00	100	\$3,500
619.01	BASIC WORK ZONE TRAFFIC CONTROL	LS	\$30,000.00	1	\$30,000
625.01	SURVEY AND STAKEOUT	LS	\$8,000.00	1	\$8,000
641.3400042	MAINTENANCE CLEANING AND WASHING OF BRIDGES, WEATHERING STEEL BRIDGES	EA	\$40,000.00	1	\$40,000
670.2003	GALVANIZED STEEL CONDUIT	FT	\$7.00	900	\$6,300
670.2606	RIGID PLASTIC CONDUIT	FT	\$5.00	9900	\$49,500
670.7004	SINGLE CONDUCTOR CABLE NO. 12 GAGE	FT	\$1.50	3000	\$4,500
670.7002	SINGLE CONDUCTOR CABLE NO. 6 GAGE	FT	\$2.00	1500	\$3,000
670.90	RELOCATE LAMPPOST ASSEMBLY	EA	\$2,000.00	3	\$6,000
685.11	WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES (20 MILS)	FT	\$0.50	1500	\$750
685.12	YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES (20 MILS)	FT	\$0.50	3000	\$1,500
699.040001	MOBILIZATION	LS	\$82,128.00	1	\$82,128

TOTAL ESTIMATED BASE CONSTRUCTION COST \$2,135,328

20% CONTINGENCY \$427,066

TOTAL ESTIMATED CONSTRUCTION COST \$2,563,000



# **APPENDIX D**

## **Inspection Reports**

**Inspection Date: 8/16/2010****RC: 43 BIN: 2211310****Bridge Ratings****Carried: DRIVING PARK AVE****Crossed: ROCH.G&E.SERV. RD****CheckValue: 1,769,540,071**

Inspection Agency: 13 - Consultant      Type of Inspection: 1 - BIENNIAL  
GTMS: 311 -- Steel - Arch - Deck  
POSTINGS: See Gen Rec Page 1 for Postings at time of inspection.  
Further Investigation Needed: No  
State Highway Number: 000000      Milepoint: 1.99      AADT/Yr: 17309 / 1997  
Orientation: 3 - East      Political Unit: 2048 - City of ROCHESTER      Year Built: 1988  
Total Spans: 4      Ramp Bridge Attached To Span: NA      BIN: NA  
General Recommendation: 5      Computed Condition Rating: 4.972

**Abutment Ratings:**

	<b>Beg Abut</b>	<b>End Abut</b>
Joint with Deck	5	4
Bearings, Bolts, Pads	6	6
Seats and Pedestals	6	6
Backwall	5	5
Stem (Breastwall)	6	6
Erosion or Scour	6	5
Footings	9	6
Piles	9	8
Recommendation	6	6

**Wingwall Ratings:**

	<b>Beg Abut</b>	<b>End Abut</b>
Walls	6	6
Footings	9	6
Erosion or Scour	7	7
Piles	9	8

**Channel Ratings:**

	<b>Channel</b>
Stream Alignment	7
Erosion and Scour	7
Waterway Opening	7
Bank Protection	8

**Approach Ratings:**

	<b>Approaches</b>
Drainage	5
Embankment	7
Settlement	6
Erosion	5
Pavement	5
Guide Railing	3

**Number of Flags Issued:**

RED: 0      Yellow: 0      Safety: 1

**Vulnerability Reviews Recommended: 1=Yes, 2=No, 3=NA, X=NotActive**

Hydraulic: 2	Overload: X	Steel: X
Collision: 3	Concrete: X	Seismic: X

**Inspector's Signature:****CheckValue: 1,769,540,071****Date: 8/16/2010**

Signed copy of this inspection report is available  
in the appropriate NYSDOT Regional Office

**Glenn T. Klein, PE ()**      (Inspector ID:4110070)

**Reviewed By:****Date: 9/9/2010**

Signed copy of this inspection report is available  
in the appropriate NYSDOT Regional Office

**Michael J. Peters, PE ()**      (QC ID:4110051)

**Inspection Date: 8/16/2010****RC: 43 BIN: 2211310****Span Ratings****Carried: DRIVING PARK AVE****Crossed: ROCH.G&E.SERV. RD****CheckValue: 1,769,540,071****Deck Element Ratings:**

	001	002	003	004
Wearing Surface	5	5	5	5
Curbs	4	4	4	4
Sidewalks, Fascias	5	5	5	5
Railings, Parapets	6	6	6	6
Scuppers	5	5	2	2
Gratings	8	8	8	8
Median	8	8	8	8
Mono Deck Surface	6	6	5	6

**Superstructure Ratings:**

	001	002	003	004
Structural Deck	6	7	4	6
Primary Members	6	6	5	6
Secondary Members	5	6	6	6
Paint	5	4	4	4
Joints	8	3	3	8
Recommendation	6	6	5	6

**Pier Ratings:**

	001	002	003	004
Bearings, Bolts, Pads	5	4	4	8
Pedestals	7	5	5	8
Top of Cap or Beam	7	5	5	8
Stem Solid Pier	8	6	6	8
Cap Beam	7	6	6	8
Pier Columns	6	6	6	8
Footings	9	6	6	8
Erosion or Scour	7	7	7	8
Piles	8	8	8	8
Recommendation	7	6	6	8

**Utility Ratings:**

	001	002	003	004
Lighting	7	8	7	7
Sign Structure	8	8	8	8
Utilities and Support	9	9	9	9

**Field Notes:**

<b>Field Date</b>	<b>Arrival</b>	<b>Departure</b>	<b>Temp (C)</b>	<b>Temp (F)</b>	<b>Weather Conditions</b>
8/16/2010	7:00:00 AM	7:00:00 PM		80	sunny

**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Notes**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

**Note ID: 431022113100006**

Beg Abut -- Abutment: Backwall -- Rated 5, Was 5

End Abut -- Abutment: Backwall -- Rated 5, Was 5

Referenced Photos: "4", "3"

2010 - The begin and end abutment backwalls exhibit similar minor cracking and efflorescence staining in Bays 1, 2, 6, and 7. Concrete generally remains solid. Backwalls remain rated 5. Bays 3 through 5 would rate 6.

**Note ID: 431022113100002**

Approaches: Guide Railing -- Rated 3, Was 5

Referenced Photos: "6", "7"

2010 - The end left guide railing has impact damage near the terminus. Three posts and the end anchor are bent and detached from the rail. The rail is kinked and bent approximately 1' out of alignment. The disconnected end assembly projects approximately 1' onto the sidewalk. Rating is lowered to 3, and Safety Flag 43100033 is issued due to this condition.

The end right guide railing has impact damage. The end anchor, 1st post, and 5th post are bent and disconnected from the rail. Other posts have minor bends but remain connected. The rail remains reasonably well aligned. This quadrant would rate 4.

Other quadrants would rate 5 or better.

**Note ID: 431022113100012**

Span 001 -- Deck Elements: Curbs -- Rated 4, Was 5

Span 002 -- Deck Elements: Curbs -- Rated 4, Was 5

Span 003 -- Deck Elements: Curbs -- Rated 4, Was 5

Span 004 -- Deck Elements: Curbs -- Rated 4, Was 6

Referenced Photos: "8", "9"

2010 - The bedding mortar beneath the granite curb is crumbling or missing up to 3" deep at sporadic locations in all spans. Some loose curb sections have been reset previously, but all original bedding mortar appears to be deteriorating. Overall, approximately 50% of the curb length on both sides has soft or missing bedding mortar. Isolated curb sections are cracked and slightly separated from the sidewalk, but no loose sections were found at this time. Curb ratings are lowered to 4.

**Note ID: 431022113100013**

Span 001 -- Deck Elements: Railings, Parapets -- Rated 6, Was 5

Span 002 -- Deck Elements: Railings, Parapets -- Rated 6, Was 6

Span 003 -- Deck Elements: Railings, Parapets -- Rated 6, Was 6

Span 004 -- Deck Elements: Railings, Parapets -- Rated 6, Was 6

Referenced Photos: "10"

2010 - The bridge railing is in good condition in all spans, and no significant defects were found. The top rail has been painted since the previous inspection. The Span 1 rating is raised to 6. Other spans remain rated 6.

**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Notes**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

**Note ID: 431022113100016**

Span 001 -- Superstructure: Primary Members -- Rated 6, Was 5

Span 002 -- Superstructure: Primary Members -- Rated 6, Was 5

Span 004 -- Superstructure: Primary Members -- Rated 6, Was 5

Referenced Photos: "17", "22", "21"

2010 - The girders in Spans 1, 2, and 4 are in good structural condition. Localized deterioration of the weathering steel patina is reflected in the paint item rating. Corrosion does not appear structurally significant at this time. The previous rating is considered somewhat harsh and is raised to 6.

**Note ID: 43102211310000E**

Span 001 -- Utilities: Utilities and Support -- Rated 9, Was 5

Span 002 -- Utilities: Utilities and Support -- Rated 9, Was 5

Span 003 -- Utilities: Utilities and Support -- Rated 9, Was 5

Span 004 -- Utilities: Utilities and Support -- Rated 9, Was 5

Referenced Photos:

2010 - Record plans indicate that numerous steel conduits are encased in the concrete sidewalks on both sides of the bridge, but these are not visible for inspection. There are no other utilities supported by the bridge. No photo. Rating is changed to 9.

**Note ID: 43102211310000A**

Span 002 -- Superstructure: Paint -- Rated 4, Was 4

Referenced Photos: "17", "22"

2010 - The weathering steel patina is rated under this item. The Span 2 girder ends and diaphragms exhibit minor delaminations beneath the end joint in Bays 1 and 7. Conditions are worst on Girders 2 and 7. Interior girder ends have minor surface scaling near the bearing areas. The remainder of the Span 2 framing appears to be weathering satisfactorily.

**Note ID: 43102211310000B**

Span 002 -- Superstructure: Joints -- Rated 3, Was 4

Span 003 -- Superstructure: Joints -- Rated 3, Was 3

Referenced Photos: "25", "26", "27", "28"

2010 - The joints at Piers 2 and 3 are in fair condition above deck. The seal is intact and there is minor corrosion on the steel armor. There is evidence of leakage below the curb lines, contributing to deterioration of weathering steel below deck. Pier 2 joint rating is lowered to 3. Pier 3 joint remains rated 3.

**Note ID: 431022113100018**

Span 002 -- Pier: Bearings, Bolts, Pads -- Rated 4, Was 4

Referenced Photos: "29"

2010 - The Span 2 end expansion bearings at Pier 2 exhibit moderate corrosion at G1, G2, G7, and G8. Conditions are worst at G2. However, the stainless steel sliding surfaces appear fully functional and the bearing position is normal.



**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Notes**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

**Note ID: 431022113100018 - continued**

The Span 3 begin expansion bearings at Pier 2 exhibit minor corrosion and would rate 5.

The arch thrust bearings would rate 6. Monitoring for "wear" on the 12.5" diameter bearing pins has shown no change since 1998. Detailed monitoring of these pins is not warranted and documentation is discontinued in this inspection.

**Note ID: 431022113100010**

Span 002 -- Pier: Pedestals -- Rated 5, Was 4

Span 003 -- Pier: Pedestals -- Rated 5, Was 4

Referenced Photos: "31", "30"

2010 - 2" thick mortar pads beneath the arch bearings extend approximately 8" beyond the edges of the masonry plate. These unreinforced edges are prone to cracking and delaminating, and large portions are missing at several locations. However, there is no undermining of the masonry plates, and the loss of this material does not appear structurally significant.

The concrete pedestals at the top of Piers 2 and 3 are in good structural condition.

The previous rating is considered too harsh and is raised to 5.

**Note ID: 431022113100017**

Span 002 -- Pier: Top of Cap or Beam -- Rated 5, Was 6

Span 003 -- Pier: Top of Cap or Beam -- Rated 5, Was 6

Referenced Photos: "31", "32", "33"

2010 - There is debris on the top pier caps from previous joint repairs at Piers 2 and 3. Debris is up to 6" thick near the left side. The pier caps remain in good structural condition.

The top of the large concrete thrust blocks at the base of the arch are also rated under this item. The Pier 2 thrust block for Arch 4 has a 3 sf hollow-sounding area at the end right corner. Other thrust blocks have isolated minor cracks.

Ratings are lowered to 5.

**Note ID: 431022113100000**

Span 003 -- Deck Elements: Scuppers -- Rated 2, Was 2

Referenced Photos: "11", "12", "13"

2010 - At the beginning of Span 3, the downspouts are disconnected from the right and left scuppers. The downspout pipes appear to have slipped downward 4 to 6" and separated at coupler. Drainage may splash spill onto the pier columns. Remains rated 2. Otherwise, the Span 3 scuppers are all open and functional.

**Note ID: 431022113100009**

Span 003 -- Superstructure: Structural Deck -- Rated 4, Was 4

Referenced Photos: "15", "16"

2010 - The Span 3 deck exhibits transverse cracking in negative moment regions near most

**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Notes**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

**Note ID: 431022113100009 - continued**

floorbeams, and at other sporadic locations. Below deck, leakage is evident at crack locations. The SIP forms are corroded and moisture is spilling onto the structural steel, resulting in accelerated deterioration. Refer to Deck Deterioration Plan. The extent of corrosion on the SIP forms has increased slightly since 2006. Structural deck remains rated 4.

**Note ID: 431022113100015**

Span 003 -- Superstructure: Primary Members -- Rated 5, Was 5

Referenced Photos: "18", "19", "20"

2010 - Span 3 intermediate floorbeams 3, 7, 9, 12, and 14 exhibit minor delaminations on the web and flanges due to deck leakage. Calliper measurements on the bottom flange of FB 7 and FB 9 indicate section losses in the range of 3 to 6%, which appears reasonably representative of other noted locations. Stringer ends framing into these floorbeams also exhibit minor web delaminations near the connections, with section losses estimated at 5%. End floorbeams 1 and 16 exhibit similar delaminations due to joint leakage, particularly beneath the curb lines. Primary members remain rated 5. The Span 3 arches, spandrel columns, and the remainder of the floor framing system have negligible section loss and would rate 6 or 7.

**Note ID: 43102211310000C**

Span 003 -- Superstructure: Paint -- Rated 4, Was 2

Referenced Photos: "18", "19", "20", "23"

2010 - The weathering steel patina is rated under this item. Span 3 Floorbeams 1, 3, 7, 9, 12, 14, and 16 exhibit surface scaling, delaminations, and minor section loss. Corrosion is localized and is attributed to leakage at expansion joints and cracks in the deck.

The light mist from the nearby waterfall does not appear to adversely affect the patina. However, there is a coating of dust on portions of the arches and spandrel bents, particularly in the begin half of the span. The surface beneath some thicker areas of built up dust exhibits light surface rust. There are also localized areas of built up dirt and debris on stiffener plates near the arch bearings. Overall 95% of the Span 3 framing appears to be weathering satisfactorily. Since deterioration is localized, the previous rating is considered too harsh and is raised to 4.

**Note ID: 431022113100019**

Span 003 -- Pier: Bearings, Bolts, Pads -- Rated 4, Was 4

Referenced Photos: "30"

2010 - The Span 4 begin fixed bearings at Pier 3 exhibit moderate corrosion at G1, G2, G7, and G8. Conditions are worst at G2, where debris is piled against the bearing. The fixed bearings appear fully functional and there is no distress.

The Span 3 end expansion bearings at Pier 3 exhibit minor corrosion and would rate 5.

The arch thrust bearings would rate 6. Monitoring for "wear" on the 12.5" diameter bearing pins has shown no change since 1998. Detailed monitoring of these pins is not warranted and documentation is discontinued in this inspection.



**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Notes**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

**Note ID: 431022113100001**

Span 004 -- Deck Elements: Scuppers -- Rated 2, Was 2

Referenced Photos: "14"

2010 - At the beginning of Span 4, the right scupper downspout is separated below the capbeam at the begin left corner of the right column. Drainage may splash spill onto the pier column. Remains rated 2. Otherwise, the Span 4 scuppers are all open and functional.

**Note ID: 43102211310000D**

Span 004 -- Superstructure: Paint -- Rated 4, Was 4

Referenced Photos: "21", "24"

2010 - The weathering steel patina is rated under this item. The Span 4 girder ends and diaphragms exhibit surface scaling and minor delaminations beneath the begin and joints in Bays 1, 2, 6, and 7. The ends of the girders and diaphragms are painted near the end abutment, and these areas would rate 5. The remainder of the Span 4 framing appears to be weathering satisfactorily and would rate 6.

**Note ID: 431022113100011**

End Abut -- Abutment: Joint with Deck -- Rated 4, Was 5

Referenced Photos: "1", "2"

2010 - The end abutment joint header is cracked along the approach slab side of the joint and there is minor spalling along the armor angles. The seal appears intact and functional, but minor leakage is evident near the curb lines from staining below deck.

**Note ID: 431022113100005**

End Abut -- Abutment: Footings -- Rated 6, Was 9

Referenced Photos: "5"

2010 - The end abutment footing is exposed across the full width of the abutment, with a maximum vertical reveal of approximately 3'. The footing is in good condition. Rating is changed from 9 to 6.

**Note ID: 431022113100014**

End Abut -- Wingwalls: Footings -- Rated 6, Was 9

Referenced Photos: "5"

2010 - The end left wingwall footing is exposed, with a maximum vertical reveal of approximately 2.5' near the abutment. The footing is in good condition. Rating is changed from 9 to 6.

**Note ID: 431022113100004**

End Abut -- Wingwalls: Piles -- Rated 8, Was 9

Referenced Photos:

2010 - Record plans indicate that the end abutment and wingwalls are founded on rock, and rock is visible along the edge of the footing. Wingwall piles rating is changed to 8. No photo.

**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Photos in Photo Number Order**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

End abutment joint,  
looking right

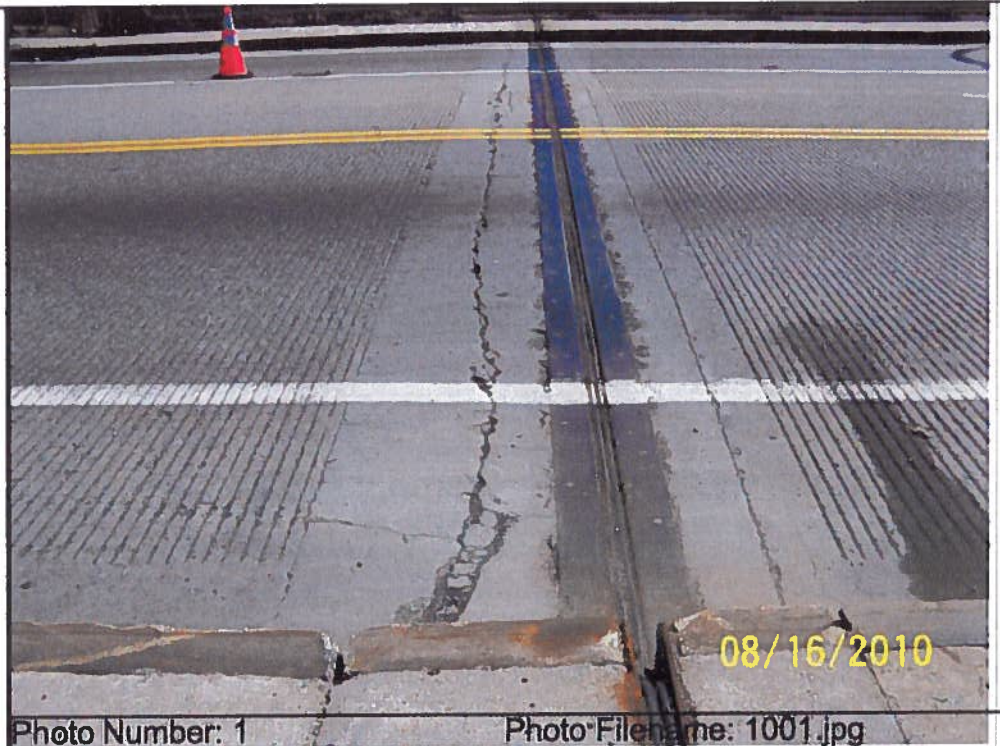


Photo Number: 1

Photo Filename: 1001.jpg

End abutment joint at G2



Photo Number: 2

Photo Filename: 1002.jpg



**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Photos in Photo Number Order**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

**End backwall in Bay 1**



**Photo Filename: 1003.jpg**

**Begin Backwall in Bay 6**



**Photo Number: 4**

**Photo Filename: 1004.jpg**



**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Photos in Photo Number Order**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

End abutment, note  
footing exposure



Photo Number: 5

Photo Filename: 1005.jpg

End left guide railing



Photo Number: 6

Photo Filename: 1006.jpg



Inspection Date: 8/16/2010

RC: 43 BIN: 2211310

Inspection Photos in Photo Number Order

Carried: DRIVING PARK AVE

Crossed: ROCH.G&E.SERV. RD

CheckValue: 1,769,540,071

End right guide railing



Photo Number: 7

Photo Filename: 1007.jpg

Right curb near begin,  
showing area of missing  
mortar (typical of all  
spans)

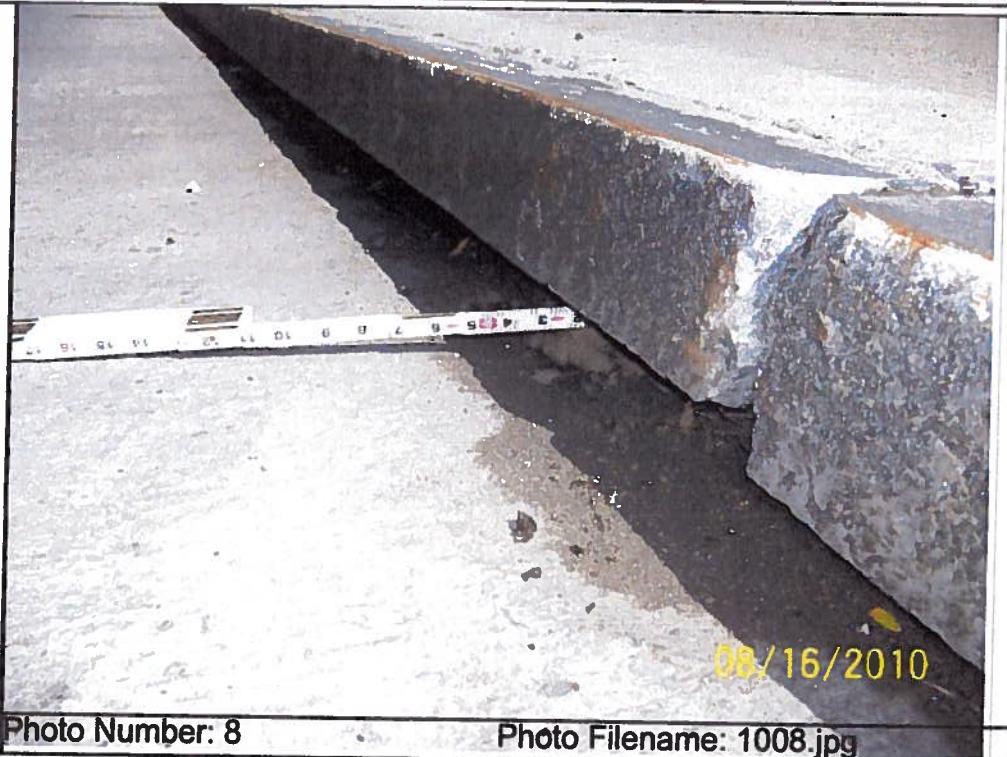


Photo Number: 8

Photo Filename: 1008.jpg



**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Photos in Photo Number Order**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

Span 3 left curb,  
showing typical curb  
repair area. Sections  
adjacent to repair have  
missing  
bedding mortar.



Photo Number: 9

Photo Filename: 1009.jpg

Span 1 right rail (typical  
all spans)

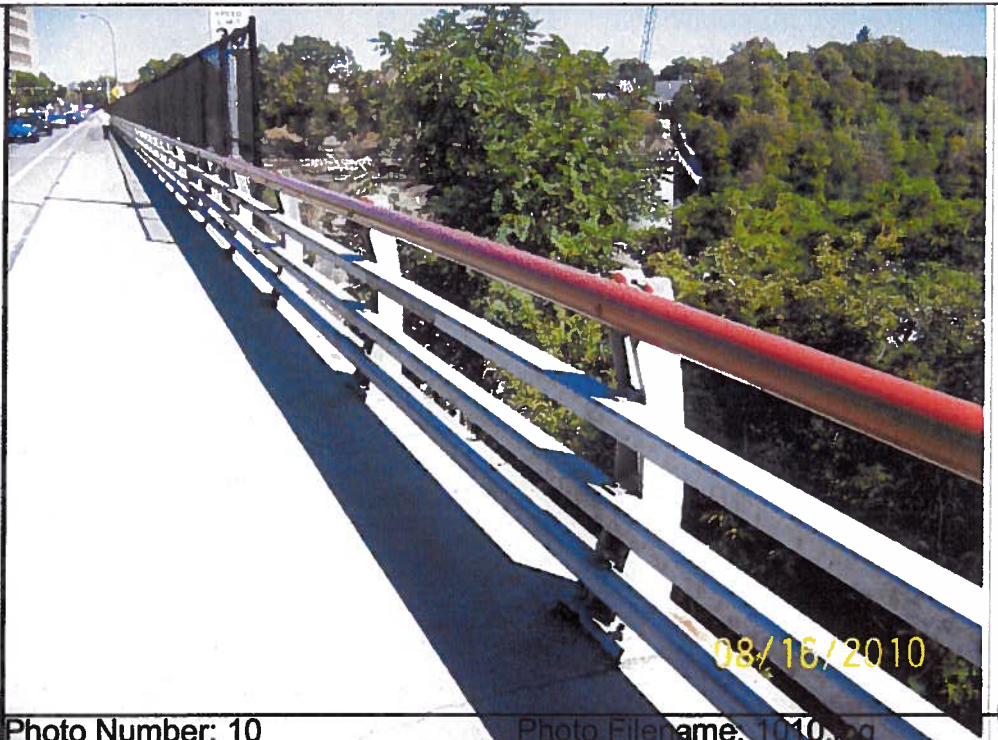


Photo Number: 10

Photo Filename: 1010.jpg



**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Photos in Photo Number Order**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

Span 3 begin right  
downspout at end side of  
Pier 2



Span 3 begin right  
scupper outlet





**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Photos in Photo Number Order**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

Span 3 begin left  
scupper outlet



Photo Number: 13

Photo Filename: 1013.jpg

Span 4 begin right  
downspout at begin side  
of Pier 3



Photo Number: 14

Photo Filename: 1014.jpg



**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Photos in Photo Number Order**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

Typical Span 3 deck  
crack, looking left near  
end of span



Span 3 typical below  
deck condition, showing  
end side of FB 12





**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Photos in Photo Number Order**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

Span 1 and 2 general  
condition



Photo Number: 17

Photo Filename: 1017.jpg

Span 3, FB 7 end side  
near center



Photo Number: 18

Photo Filename: 1018.jpg

**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Photos in Photo Number Order**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

Span 3 end floorbeam



Photo Number: 19

Photo Filename: 1019.jpg

Span 3 arch, looking from end left



Photo Number: 20

Photo Filename: 1020.jpg



**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Photos in Photo Number Order**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

Span 4 general condition



Photo Number: 21

Photo Filename: 1021.jpg

Span 2 G2 at end.



Photo Number: 22

Photo Filename: 1022.jpg



**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Photos in Photo Number Order**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

Span 3 arch near begin,  
showing coating of dust  
on weathering steel



Photo Number: 23

Photo Filename: 1023.jpg

Span 4 G2 at begin



Photo Number: 24

Photo Filename: 1024.jpg



Inspection Date: 8/16/2010

RC: 43 BIN: 2211310

Inspection Photos in Photo Number Order

Carried: DRIVING PARK AVE

Crossed: ROCH.G&E.SERV. RD

CheckValue: 1,769,540,071

Pier 1 joint, looking right

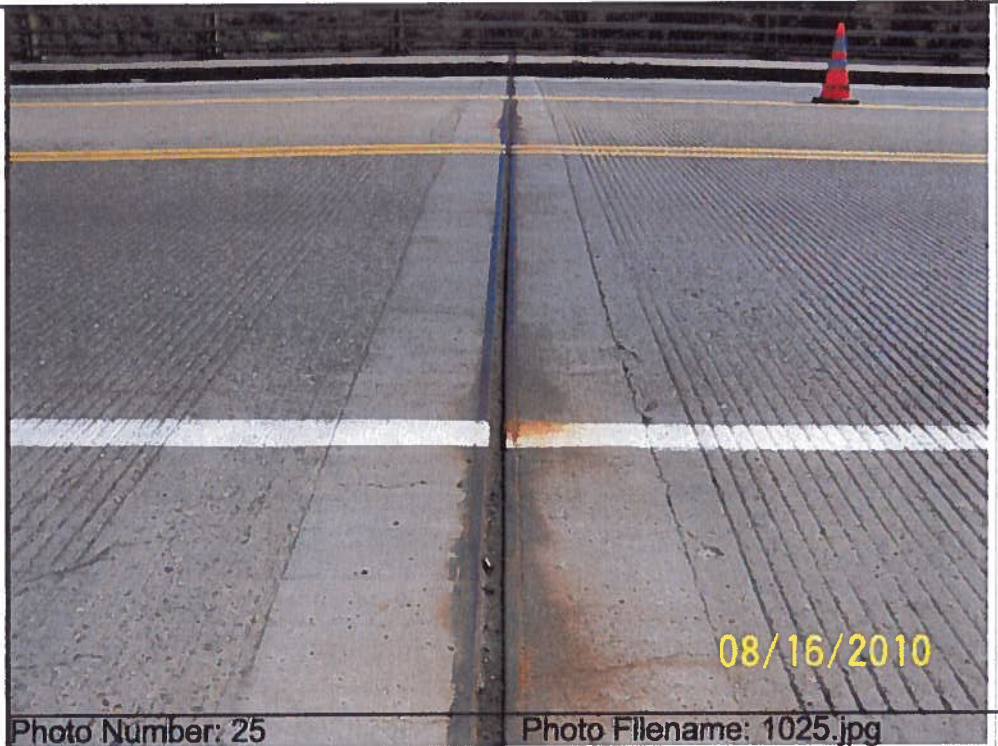


Photo Number: 25

Photo Filename: 1025.jpg

Pier 1 joint, below deck  
near left curb line



Photo Number: 26

Photo Filename: 1026.jpg



**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Photos in Photo Number Order**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

Pier 2 joint, looking right



Pier 2 joint, below deck  
near left curb line





**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Photos in Photo Number Order**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

Span 2 end bearing at  
G2



**Photo Number: 29**

**Photo Filename: 1029.jpg**

Span 2 begin bearing at  
G2



**Photo Number: 30**

**Photo Filename: 1030.jpg**



**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Photos in Photo Number Order**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

Span 3 Arch 4 bearing at  
begin (Arch 3 at end is  
similar)



Photo Number: 31

Photo Filename: 1031.jpg

Top of Pier 2 in Bay 7



Photo Number: 32

Photo Filename: 1032.jpg

**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Photos in Photo Number Order**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

Top of Pier 3 in Bay 1



**No Photo**



Inspection Date: 8/16/2010

RC: 43 BIN: 2211310

## Inspection Sketches in Sketch SysID Order

Carried: DRIVING PARK AVE

Crossed: ROCH.G&E.SERV. RD

CheckValue: 1,769,540,071

Sketch ID: 431022113100000

Sketch Filename: 10PhotoLocation.tif

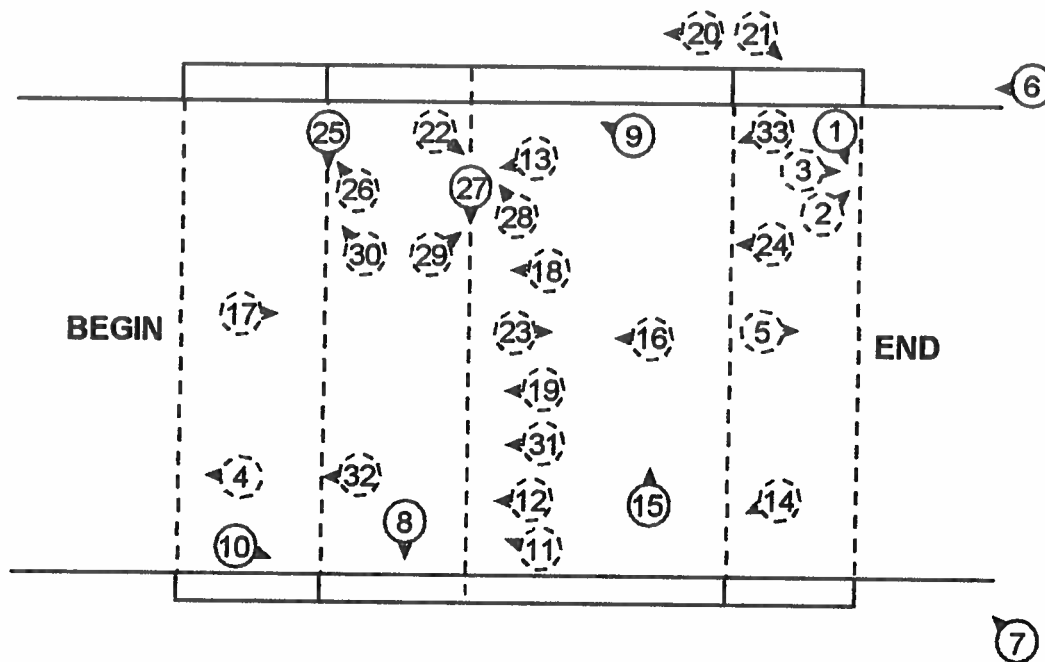
General Sketch for Bridge

Referenced Photos:

Photo Location Plan

### PHOTO LOCATION PLAN

BIN 2211310  
8/16/2010



○ - PHOTO ABOVE DECK

○ - PHOTO BELOW DECK

Inspection Date: 8/16/2010

RC: 43 BIN: 2211310

## Inspection Sketches in Sketch SysID Order

Carried: DRIVING PARK AVE

Crossed: ROCH.G&E.SERV. RD

CheckValue: 1,769,540,071

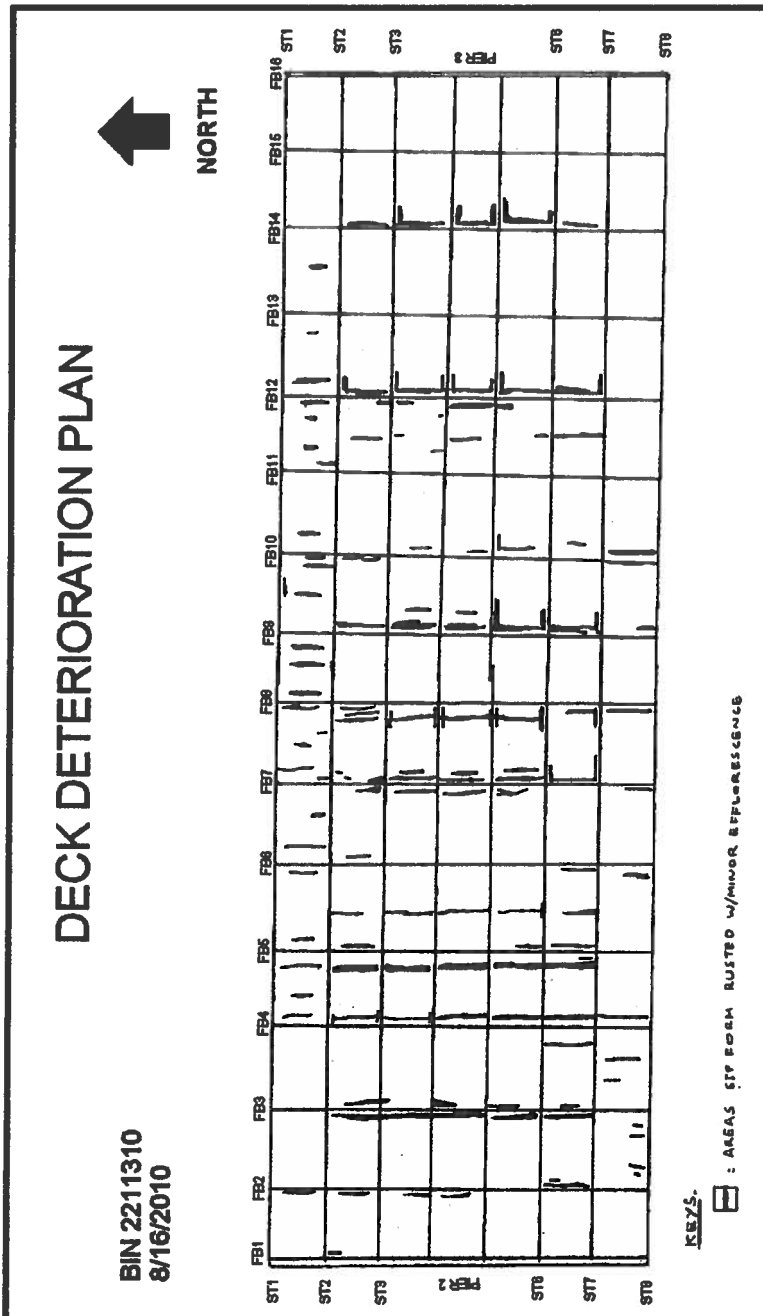
Sketch ID: 431022113100001

Sketch Filename: 10DeckDeterioration.tif

Span 003 -- Superstructure: Structural Deck -- Rated 4, Was 4

Referenced Photos: "15", "16"

Deck Deterioration Plan



**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Gen. Rec., Postings, Federal Ratings, etc.**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

**Overall Condition:**

GENERAL RECOMMENDATION: 5

Computed Condition Rating: 4.972

**Problems Requiring Action:**

NO Further Investigation Needed

SAFETY Flag(s) Issued

**POSTINGS:**

Inspector Confirmed existing Posting data as correct.  
Posted Vertical Clearance ON the bridge is: No Posting  
Posted Vertical Clearance UNDER the bridge is: No Posting  
No Load Restriction is posted on this bridge

**Overloads Observed:**

NO Overload Vehicles were observed on this bridge

**FEDERAL RATINGS:**

NBI Deck Condition: 5  
NBI Superstruct Condition: 7  
NBI Substruct Condition: 7  
NBI Channel Condition: 9  
NBI Culvert Condition: N

**Diving Inspection Needs:**

Diving Inspection Required? No

Date of Last Diving Inspection: No Date

**Inventory Problems:**

Inventory Problems Exist? No

**Miscellaneous:**

Time Required to Inspect Bridge: 12 Hours

Lane Closure Needs: By Contract for 8 Hours

No Railroad Flagging Required

There is a Pedestrian Fence 8.00 Feet High

No Snow Fence

The BIN Plate is in OK condition



**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Gen. Rec., Postings, Federal Ratings, etc.**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

**Special Emphasis Inspection Required:**

Non-Redundant/Fracture Critical Members - No  
Pin and Hangers - No  
Fatigue-Prone Welds - No  
Non-Categorized Fatigue-Prone Details - No  
Other (Specified in Text) - No

**Special Emphasis Details:**

No Special Emphasis Inspection Required.

**General Notes To the Next Inspector:**

BIN plate is on the begin left stem.

A 60' UBIU was used for underbridge access. The arch bearings were not accessible from the UBIU. The Pier 3 arch bearings were accessed by exiting the UBIU adjacent to the gabion wall at end left and walking/climbing down to the bearings. The Pier 2 arch bearings were accessed by climbing down from the walkway below Span 2. A 6' stepladder is recommended at Pier 2.

**Improvements Observed:**

2010 - Sporadic loose curb sections have been repaired. The top rail of the bridge railing has been painted.

**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Review Progress and Personnel Present at Inspection**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

**Inspection Submission Status:**

Submitted to QC Engineer on: 9/8/2010  
QC Submission Number: 30401102

QC Review Completed: 9/9/2010  
QC Engineer: Michael J. Peters

Submitted to Liaison Engineer on: 9/13/2010  
Liaison Submission Number: 04018

Liaison Review Completed: 10/29/2010  
Liaison Engineer: Ikram A. Mohl

Submitted for BIIS Processing on: 10/29/2010  
BIIS Submission Number: .kp1

Current Status: Keypunched, Sent to BIIS  
Check Value: 1,769,540,071

**Personnel Present During Inspection:**

Glenn T. Klein	- Team Leader
Dawn Urbino	- Assistant Team Leader
Palmer Cox	- MPT flagger
Frank Desalio	- MPT forman
Garfield Jones	- MPT flagger
Brian Kjor	- UBIU driver

**Discovery Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Safety Flag 43100033**

**Carried: DRIVING PARK AVE Crossed: ROCH.G&E.SERV. RD**

Prompt Interim Action Recommended: No

Inspector: Klein, Glenn T.  
Flag Number: 43100033

Date Discovered: 8/16/2010  
Supersedes Flag Number: \_\_\_\_\_

**Bridge Description:**

BIN: 2211310 Carried: DRIVING PARK AVE Crossed: ROCH.G&E.SERV. RD

Region: 4 - Rochester County: 3 - Monroe  
Political Unit: 2048 - City of ROCHESTER  
Residency Code: - N/A  
Primary Owner: 42 - City  
Secondary Owner: 99 - One Agency - Listed in first subfield  
Primary Maintenance: 42 - City  
Secondary Maintenance: 99 - One Agency - Listed in first subfield  
Year Built: 1988 Not Posted For Load

Number of Spans by Type:	Num	Type	Description
	003	- 213	- Weathering Steel - Plate Girder, Multi-Girder
	001	- 224	- Weathering Steel - Arch, Deck - Open Spandrel

**Description of Flagged Condition:**

The end left guide railing has impact damage near the terminus. Three posts and the end anchor are bent and detached from the rail. The rail is kinked and bent approximately 1' out of alignment. The disconnected end assembly projects approximately 1' onto the sidewalk.

Bridge is oriented East.

1 Photos/Sketches Attached

Verbal Notifications: (For RED Flags and Safety Flags with PIA only)

To: \_\_\_\_\_ of Regional Office on \_\_\_\_\_ at \_\_\_\_\_

Signature: (a signed copy of this report will be placed in the BIN folder)

Flagged Bridge Report Completed By: Klein, Glenn T. on 8/16/2010

Flagged Bridge Report Signed By: \_\_\_\_\_ on \_\_\_\_\_  
Klein, Glenn T.

(This PDF Report Created: 11/1/2010 3:13:44 PM)



**Discovery Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Safety Flag 43100033 Attachment**

**Carried: DRIVING PARK AVE    Crossed: ROCH.G&E.SERV. RD**

1006.jpg - Attached to Safety Flag 43100033

End left guide railing



**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Inspection Access Requirements**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

**Equipment Required for Inspection**

No Access Requirement Changes Noted During This Inspection.  
This Listing is from the Inventory Database.

**ACCESS CATEGORIES FOR ENTIRE BRIDGE**

Required: Walking, Step Ladder, Extension Ladder, 60 Ft UBIU (18 m)

Required: Lane Closure, Shadow Vehicle

**ACCESS CATEGORIES FOR SPAN 1**

Required: Walking, Extension Ladder, 60 Ft UBIU (18 m)

Required: Lane Closure, Shadow Vehicle

**ACCESS CATEGORIES FOR SPAN 2**

Required: Walking, Step Ladder, 60 Ft UBIU (18 m), Lane Closure

Required: Shadow Vehicle

**ACCESS CATEGORIES FOR SPAN 3**

Required: Walking, 60 Ft UBIU (18 m), Lane Closure

Required: Shadow Vehicle

**ACCESS CATEGORIES FOR SPAN 4**

Required: Walking, Extension Ladder, 60 Ft UBIU (18 m)

Required: Lane Closure, Shadow Vehicle

**Inspection Date: 8/16/2010**

**RC: 43 BIN: 2211310**

**Culvert Measurements**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**CheckValue: 1,769,540,071**

**Culvert Measurements**

NO CULVERT DATA FOR BIN 2211310



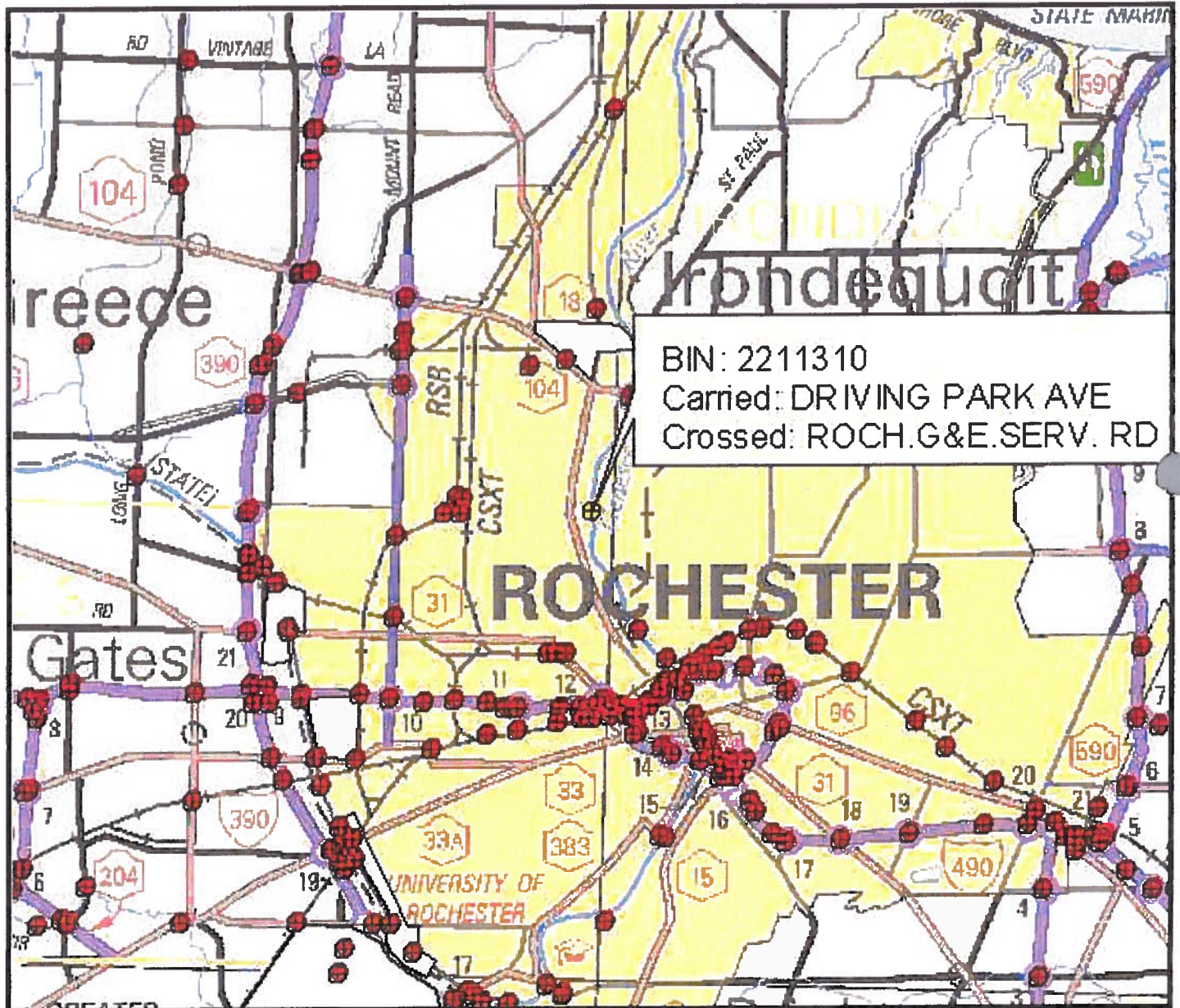
## Standard Photos

**RC: 43 BIN: 2211310**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**2211310\_LOCATION\_MAP.JPG**



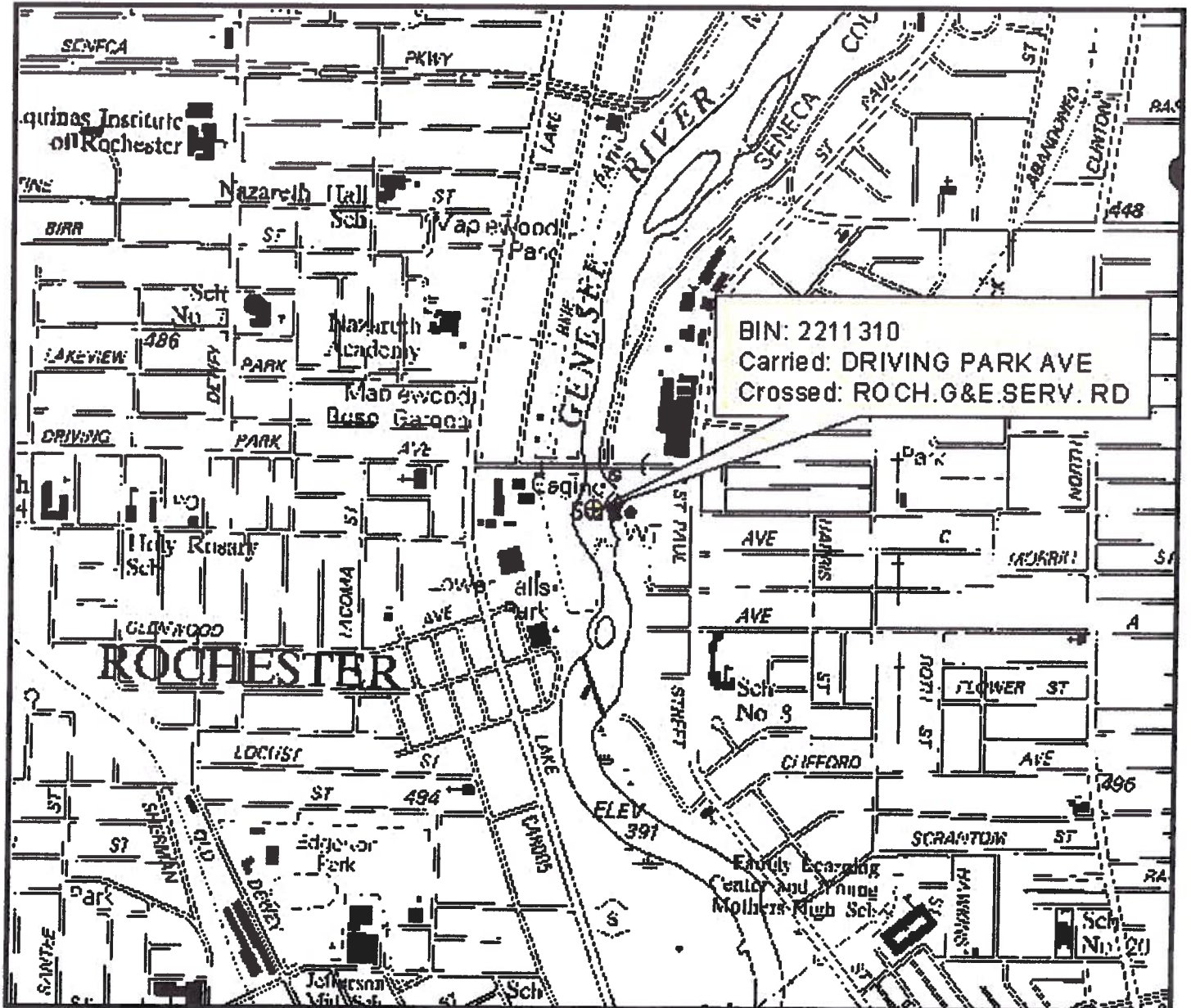
Standard Photos

RC: 43 BIN: 2211310

Carried: DRIVING PARK AVE

Crossed: ROCH.G&E.SERV. RD

2211310\_QUAD\_MAP.JPG





**Standard Photos**

**RC: 43 BIN: 2211310**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**ApproachBegin.JPG**





**Standard Photos**

**RC: 43 BIN: 2211310**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**ApproachEnd.JPG**



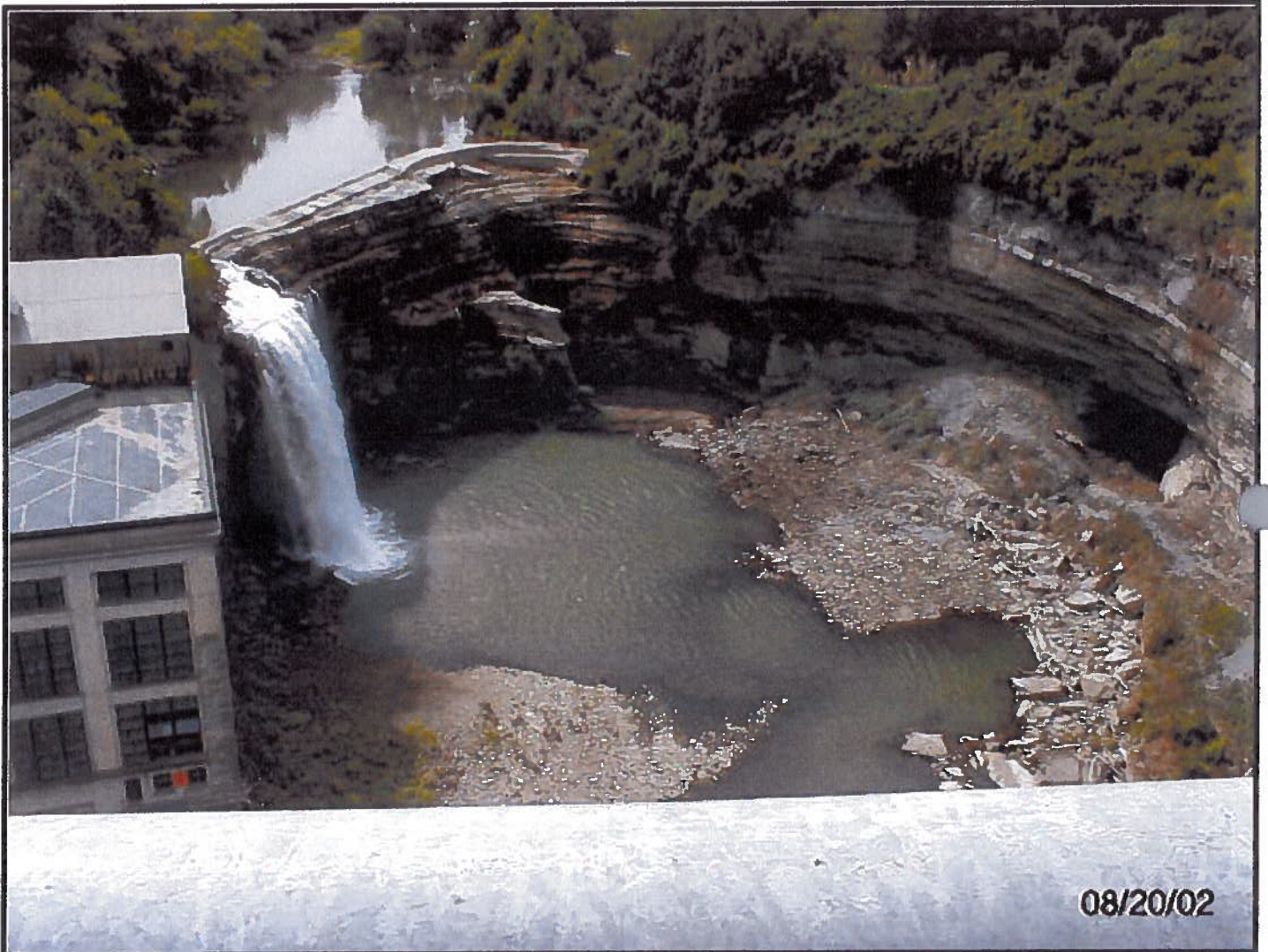
**Standard Photos**

**RC: 43 BIN: 2211310**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**ChaneelUpstreamRight.JPG**





**Standard Photos**

**RC: 43 BIN: 2211310**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**ChannelDownstreamLeft.JPG**





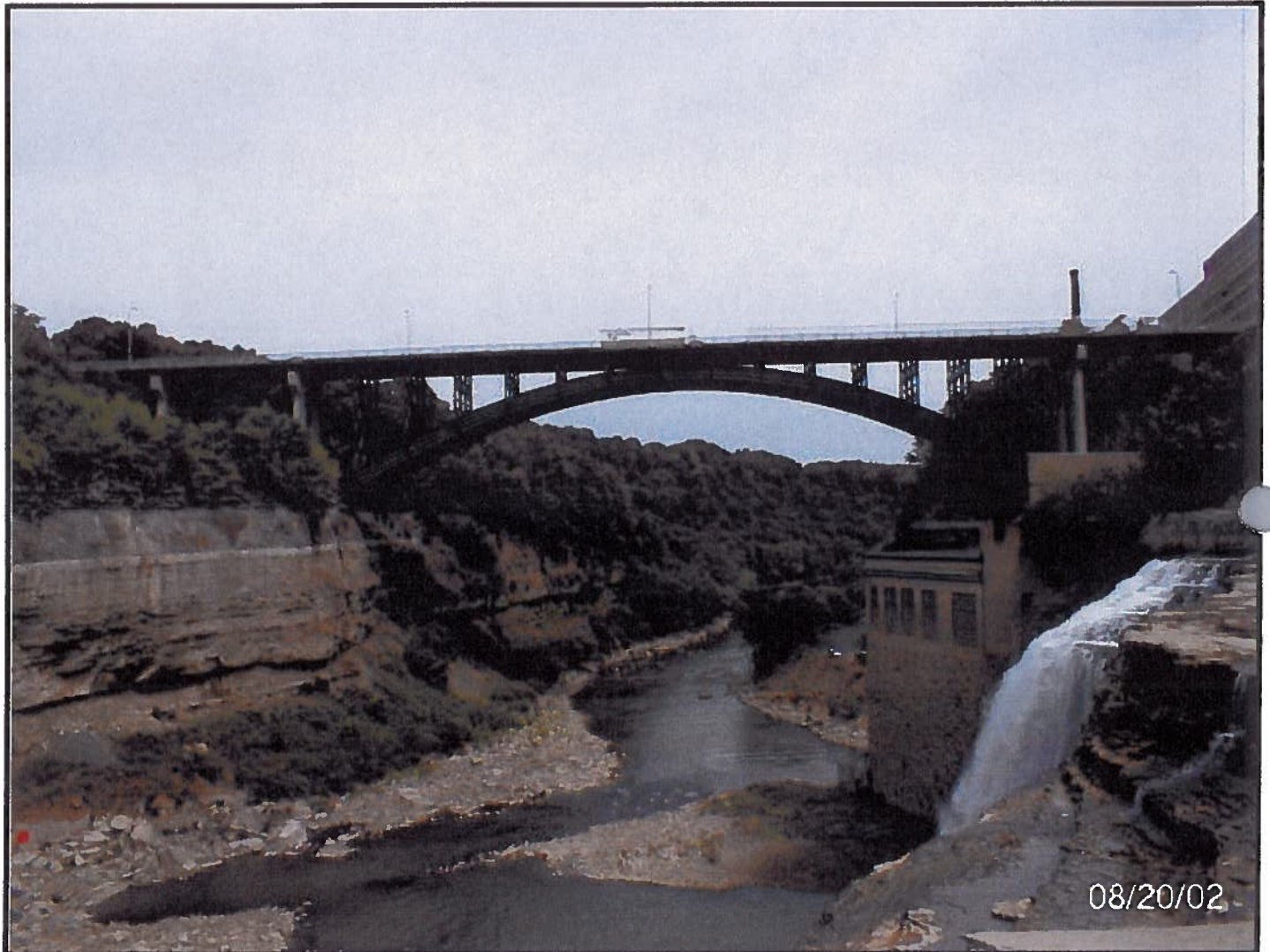
**Standard Photos**

**RC: 43 BIN: 2211310**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**ElevationSpans1,2,3&4Right.JPG**



**Standard Photos**

**RC: 43 BIN: 2211310**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**FramingSpan1TypicalSpans2&4.JPG**





**Standard Photos**

**RC: 43 BIN: 2211310**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**FramingSpan1TypicalSpans2,3,&4.JPG**





**Standard Photos**

**RC: 43 BIN: 2211310**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**FramingSpan3.JPG**





**Standard Photos**

**RC: 43 BIN: 2211310**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**Pier1.JPG**



**Standard Photos**

**RC: 43 BIN: 2211310**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**Pier2BearingAreaTypicalPier3.JPG**





**Standard Photos**

**RC: 43 BIN: 2211310**

**Carried: DRIVING PARK AVE**

**Crossed: ROCH.G&E.SERV. RD**

**UnderDeckSpan1TypicalSpans2,3&4.JPG**



FY 12 - 13 CASH CAPITAL CODES	

TRANSPORTATION & INFRASTRUCTURE			Project ID Description		Fund C:		Appropriation	
Org. Code	Org. Description	Object Code	Obj. Code Description	Project ID			(original budget)	
GF250607	DES - Street Design	731000	Eng/Arch Design Service	13001	Arterial Improvements	x	200,000	
GF250603	DES - Construction	733000	Trans. Construction	13002	Street Rehabilitation	x	470,000	
WT250603	DES - Construction	733000	Trans. Construction	13002	Street Rehabilitation	xw	58,000	
GF251332	DES - Special Svcs	700009	Maint. & Repair Street	13003	Street Surface Treatment	x	300,000	
LW251331	DES - Road Surface Treatment	700009	Maint. & Repair Street	13003	Street Surface Treatment	xlw	915,000	
LW250607	DES - Street Design	736000	Sidewalks Haz	13005	Sidewalk Maintenance	xlw	2,138,000	
LW250603	DES - Construction	737000	Sidewalks Ramps	13005	Sidewalk Maintenance	xlw	270,000	
GF250608	DES - Street Lighting	733002	Trans. Construction/Safety	20006	Street Lighting System	x	70,000	
GF250608	DES - Street Lighting	733003	Trans. Construction/Standard Allocation	13006	Street Lighting System	x	116,000	
GF250608	DES - Street Lighting	733000	Trans. Construction	13006	Street Lighting System	x	228,000	
GF250608	DES - Street Lighting	733000	Trans. Construction	13101	Street Lighting Improvements	x	325,000	
GF250608	DES - Street Lighting	733000	Trans. Construction	13102	Brownroft/Nunda Modernization	x	400,000	
GF250609	DES - Structures	733000	Trans. Construction	13007	Bridges & Structures	x	526,000	
TOTAL TRANSPORTATION & INFRASTRUCTURE							6,016,000	

<u>Org. Code</u>	<u>Org. Description</u>	<u>Object Code</u>	<u>Obj. Code Description</u>	<u>Project ID</u>	<u>PUBLIC WATERFRONT</u> <u>Project Description</u>	<u>Fund C:</u>	<u>Appropriation</u>
GF250607	DES - Street Design	733003	Trans. Construction/Standard Allocation	13009	Port of Rochester	x	25,000
GF250609	DES - Structures	732000	General Development	13103	Johnson and Seymour Mill Race	x	250,000
GF250607	DES - Street Design	732002	Construction Site	13104	Boat Launch Relocation Riverfront Development III	x	18,000
GF250607	DES - Structures	732000	Site Construction	13105	RGE Gas Main Upgrades	x	1,250,000
					<b>TOTAL PUBLIC WATERFRONT</b>		<b>1,543,000</b>

[illegible]

Org. Code	Org. Description	Object Code	Obj. Code Description	Project ID	Project Description	Fund C:	Appropriation
GF402010	FIRE - Hose and Supply Depot	700003	Small Equipment	13017	Small Equipment	x	638,000
GF402010	FIRE - Hose and Supply Depot	702001	Small Equipment	13019	Technology	x	40,000
GF352035	Police - Police Budget	700003	Small Equipment	13017	Small Equipment	x	141,000
GF402010	FIRE - Hose and Supply Depot	703001	Motor Vehicles	13064	Fire Motor Equipment	x	99,000
					<b>TOTAL PUBLIC SAFETY</b>		<b>918,000</b>

Org. Code	Org. Description	Object Code	Obj. Code Description	Project ID	PARKS & OPEN SPACE Project Description	Fund C:	Appropriation
GF250602	DES - Construction	733000	Trans. Construction	13107	Riverway Trail CMAQ	x	220,000
GF250601	DES - Street Design	733002	Trans. Construction/Safety	13108	Bicycle Enhancement CMAQ	x	48,000
GF251342	DES - Forestry	700004	Agricultural & Horticultural	13021	Urban Forest	x	450,000
GF251342	DES - Forestry	700004	Agricultural & Horticultural	13109	Ash Tree	x	62,000
GF250606	DES - Architectural Services	732002	Construction Site	13110	Charles Carroll Plaza	x	250,000
GF500101	DRYS - Commissioner's Office	742000	Non Right of Way Maintenance	13023	Recreation	x	788,000
GF250602	DES - Construction	732003	Construction Site	13023	Recreation	x	125,000
PM500525	DRYS - Public Market	730002	Annual Allocation	13024	Public Market	xpm	44,000
CM250103	DES - Cemeteries	744000	Memorialization	20025	Cemeteries	xc	80,000
CM250103	DES - Cemeteries	732000	General Development	20025	Cemeteries	xc	212,000
CM250103	DES - Cemeteries	732001	Interment Space Development	20025	Cemeteries	xc	50,000
					TOTAL PARKS & OPEN SPACE		2,329,000

ECONOMIC DEVELOPMENT			Project Description		Fund C:	Appropriation
Org. Code	Org. Description	Object Code	Obj. Code Description	Project ID		
GF250609	DES - Structures	700005	Grounds Maintenance Equipment	13111	Center City Two-Way Conversion	x
GF200520	NBD - Project Development	725004	Site Preparation	13026	Downtown Development	x
GF250601	DES - Engineering SVC	733001	General Dev/Streets	13072	Downtown Development	x
PK250601	DES - Engineering SVC	733001	General Dev/Streets	13072	Downtown Development	x
GF200520	NBD - Project Development	725001	Acquisition	13029	Land Acquisition	x
GF200520	NBD - Project Development	725002	Demolition	13030	Demolition	x
GF250102	DES - Environmental Quality	729000	Env. Remediation	13031	Environmental Compliance & Remediation	x
GF250102	DES - Environmental Quality	729000	Env. Remediation	13112	Environmental Compliance & Remediation - Pattonwork	x
GF250102	DES - Environmental Quality	729000	Env. Remediation	13113	Environmental Compliance & Remediation - Port of Rd	x
GF250102	DES - Environmental Quality	729000	Env. Remediation	13114	Environmental Compliance & Remediation - Vacuum C	x
GF200515	NBD - Housing	725006	Home Purchase Program	13032	Housing	x
GF200515	NBD - Housing	725001	Acquisition	13032	Housing	x
GF250607	DES - Street Design	733001	General Dev/Streets	13115	Dewey Avenue Intersection Realignment	x
GF200515	NBD - Housing	726001	Acquisition	13082	Focused Investment	x
					TOTAL ECONOMIC DEVELOPMENT	9,691,000



<u>Org. Code</u>	<u>Org. Description</u>	<u>Object Code</u>	<u>Obj. Code Description</u>	<u>Project ID</u>	<u>ARTS &amp; CULTURE</u>	
					<u>Project Description</u>	<u>Fund C: Appropriation</u>
LB450505	Library - Branch Administration	701000	Library Material	13034	Library Books & Materials	xi 508,000
					<b>TOTAL ARTS &amp; CULTURE</b>	<b>508,000</b>

<u>Code</u>	<u>Org. Description</u>	<u>Object Code</u>	<u>Obj. Code Description</u>	<u>Project ID</u>	<u>Project Description</u>	<u>Appropriation</u>
WM200530	NBD - War Memorial	730002	Standard Alloc Facilities	13035	Blue Cross Arena at the War Memorial	wmx 70,000
WM200530	NBD - War Memorial	730001	Renovation/Construction	13116	Blue Cross Arena - Purge Fans	wmx 125,000
WM250606	DES - Architectural Services	730005	HVAC	13117	Blue Cross Arena	wmx 170,000
GF250601	DES - Street Design	730001	Renovation/Construction	13036	High Falls District	x 61,000
GF250609	DES - Street Design	730001	Renovation/Construction	13036	High Falls District	x 61,000
GF250609	DES - Street Design	730003	General Development Structures	13118	High Falls Festival Site Girge Wall	x 150,000
GF200510	NBD - Real Estate	730002	Standard Alloc Facilities	13038		x 15,000
					<b>TOTAL PUBLIC FACILITIES</b>	<b>652,000</b>

<u>Code</u>	<u>Org. Description</u>	<u>Object Code</u>	<u>Obj. Code Description</u>	<u>Project ID</u>	<u>Project Description</u>	<u>Fund C: Appropriation</u>
RF250606	DES - Architectural Services	730002	Annual Allocation	20041	Operations Center Complex	xf 40,000
GF251350	DES - Fleet Management	730004	Lift Replacement	13041	Operations Center Complex	x 40,000
LW251350	DES - Fleet Management	730004	Lift Replacement	13041	Operations Center Complex	xlw 40,000
RF251350	DES - Fleet Management	730004	Lift Replacement	13041	Operations Center Complex	xf 40,000
WT251350	DES - Fleet Management	730004	Lift Replacement	13041	Operations Center Complex	xw 10,000
GF250606	DES - Architectural Services	730002	Renovation/Construction	13041	Operations Center Complex	x 85,000
LW250606	DES - Architectural Services	730002	Renovation/Construction	13041	Operations Center Complex	xlw 54,000
RF250606	DES - Architectural Services	730002	Renovation/Construction	13041	Operations Center Complex	xf 54,000
LB250606	DES - Architectural Services	730002	Renovation/Construction	13042	Central Library Facilities	xl 76,000
GF250606	DES - Architectural Services	730001	Renovation/Construction	13119	Adams Street Recreation Center	x 485,000
GF250606	DES - Architectural Services	730001	Renovation/Construction	13120	Parks Maintenance Building	x 910,000
WT252007	Water Director's Office	730001	Renovation/Construction	13121	Water Bureau Garage Floor Repairs	x 300,000
GF250609	DES - Street Design	730001	Renovation/Construction	13122	Blue Cross Arena Riverwall	x 50,000
GF200510	NBD - Real Estate	730002	Annual Allocation	13045	General Rehabilitation	x 120,000
GF250606	DES - Architectural Services	730002	Annual Allocation	10345	General Rehabilitation	x 150,000
GF200510	NBD - Real Estate	730001	Renovation/Construction	13123	Midtown Facility	x 150,000
GF250606	DES - Architectural Services	730005	HVAC	13124	New York Power Authority	x 160,000
GF200520	NBD - Project Development	730001	Renovation/Construction	13031	Environmental Compliance and Remediation	x 150,000
PK250609	DES - Street Design	730002	Annual Allocation	13046	Garages	xp 500,000
GF300101	DES - Architectural Services	730002	Annual Allocation	13088	Emergency Communications Center	x 250,000
GF250606	DES - Architectural Services	730002	Annual Allocation	13088	Emergency Communications Center	x 55,000
GF250606	DES - Architectural Services	730002	Annual Allocation	13089	Fire Facilities	x 124,000
GF352035	DES - Architectural Services	730002	Annual Allocation	13090	Police Facilities	x 220,000



Org. Code	Org. Description	Object Code	Obj. Code Description	Project ID	NON-PUBLIC SAFETY FLEET	
					Project Description	Appropriation
GF251350	DES - Fleet Management	703000	Motor Vehicles	13060	Motor Equipment	x 916,000
WT251350	DES - Fleet Management	703000	Motor Vehicles	13060	Motor Equipment	xw 154,000
LW251350	DES - Fleet Management	703000	Motor Vehicles	13060	Motor Equipment	xlw 519,000
CM251350	DES - Fleet Management	703000	Motor Vehicles	13060	Motor Equipment	xc 190,000
RF251350	DES - Fleet Management	703000	Motor Vehicles	13060	Motor Equipment	rx 554,000
TOTAL NON-PUBLIC SAFETY FLEET						2,333,000



Level_1	Level_2	Level_3	Object_123	Object_456	Object	
Equipment	Equipment & Material	Office Equipment	700	001	700001	
		Furniture & Furnishings	700	002	700002	
		Small Equipment	700	003	700003	
		Agricultural & Horticultural	700	004	700004	
		Grounds Maintenance Equipment	700	005	700005	
		Audiovisual & Photo Equipment	700	006	700006	
		Street Lighting Equipment	700	007	700007	
		Other Equipment	700	008	700008	
		Maintenance & Repair, Street	700	009	700009	
		Communications	700	010	700010	
	Library Material		701	000	701000	
		Adult book	701	001	701001	
		YA book	701	002	701002	
		Children book	701	003	701003	
		Adult DVD	701	004	701004	
		Children DVD	701	005	701005	
		Adult CD	701	006	701006	
		Children CD	701	007	701007	
		ebook	701	008	701008	
		Audio book	701	015	701015	
	Information Technology Hardware	Media	701	020	701020	
		Serials	701	200	701200	
		Non-Fiction	701	300	701300	
		IT Hardware	702	001	702001	
		Duplicating Equipment	702	002	702002	
		Telephony	702	003	702003	
		Professional Services	702	004	702004	
		IT Software	702	005	702005	
	Motor Vehicles		703	000	703000	
		Light Vehicles	703	001	703001	
		Heavy Vehicles	703	002	703002	
	Energy Conservation		704	000	704000	
Infrastructure	Land/Development	Acquisition	725	001	725001	
		Demolition	725	002	725002	
		Appraisal & Legal Costs	725	003	725003	
		Site Preparation	725	004	725004	
		Asbestos Abatement	725	005	725005	
		Home Purchase Program	725	006	725006	
	Focused Investment Strategy	Owner Occupant Rehab	726	001	726001	
		Lead Assessments	726	002	726002	
		Investor Rehabilitation	726	003	726003	
		Acquisition	726	004	726004	
		Demolition	726	005	726005	
		Appraisal & Legal Costs	726	006	726006	
		Site Preparation	726	007	726007	
		Asbestos Abatement	726	008	726008	
		Home Purchase Program	726	009	726009	
		Construction/Renovations	726	010	726010	
	Lead Paint Assessments		727	000	727000	
	Facility Environmental Compliance		728	000	728000	
	Environmental Remediation		729	000	729000	
	Buildings	Renovation/Construction	730	001	730001	
		Annual Allocation	730	002	730002	
		General Development Structures	730	003	730003	
		Lift Replacement	730	004	730004	
		HVAC	730	005	730005	New
	Engineering /Architectural Design Services		731	000	731000	
	Site Construction (Public Works)	General Development	732	000	732000	
		Interment Space Development	732	001	732001	
		New Construction Site	732	002	732002	New
	Transportation Construction		733	000	733000	
		General Development Streets	733	001	733001	
			733	002	733002	
		Standard Allocation	733	003	733003	
	Traffic		734	000	734000	
	Sewer - Ancillary		735	000	735000	

Sidewalk Hazardous	736	000	736000	
Sidewalk Ramps	737	000	737000	
Right-of-Way	738	000	738000	
Bridge Rehabilitation/Construction	739	000	739000	
Water System Rehabilitation/Construction	740	000	740000	
	Water Main & Ex. Improv	740	001	740001
	Water Main Cleaning & Lining	740	002	740002
	Hydrant & Valve Replacement	740	003	740003
	Water Meter Replacement	740	004	740004
	General System Replacement	740	005	740005
	Reservoir, Conduit & Watershed	740	006	740006
	Water Operations Center	740	007	740007
	Holly System	740	008	740008
	Hemlock Filtration Plant	740	009	740009
	Water Security	740	010	740010
Non-Right of Way Maintenance & Repair	742	000	742000	
Marina Maintenance & Repair	743	000	743000	
Memorialization	744	000	744000	

# **APPENDIX E**

## **Environmental Information**



## Environmental Checklist

PIN: 4755.30

DESIGNER: LaBella Associates, P.C.

DESCRIPTION: Driving Park Avenue Bridge over Genesee  
River Preventive Maintenance

ENVIRON. CONTACT: John Papponetti, P.E.

TOWN/CITY: Rochester

DATE: 02/10/2012

COUNTY: Monroe

REVISION DATE:

ENVIRONMENTAL  
CLASSIFICATION

NEPA:

Class II Automatic Categorical Exclusion

SEQA:

TYPE II

ENVIRONMENTAL ISSUE		INVOLVEMENT		FURTHER REVIEW REQUIRED	COMMENTS
		YES	NO		
1.	Parkland - State, County & Local Parks & Trails	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
2.	Parkland - Nationwide 4(f), Section 4(f), Section 6(f), Section 1010	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
3.	Historic & Archaeological Resources - General and/or Section 4(f)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	NYSDOT is reviewing Section 106 Package
4.	Natural Landmarks	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
5.	Visual Resources	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
6.	Coast Guard Bridge Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
7.	Floodplains	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Project is located in a Flood Plain, but no work is projected to impact the existing hydraulics of the bridge
8.	Wetlands - Federal	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
9.	Executive Order 11990	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
10.	Wetlands - State - Article 24 (Freshwater) or Article 25 (Tidal) Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
11.	Corps of Engineers - Section 10 or 404, Nationwide or Individual Permits	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
12.	Water Quality Certification - Section 401	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
13.	Water Quality Analysis	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
14.	Sole Source Aquifer	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
15.	SPDES Stormwater Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
16.	Wild, Scenic & Recreational Rivers - Federal or State	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
17.	Coastal Zone Management	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
18.	Critical Environmental Areas	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
19.	Endangered or Threatened Species	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Bog Turtle, American Burying Beetle & Purple Bluets - No Impact
20.	Farmland or Agricultural District	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
21.	Scenic Roads	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
22.	Air Quality Analysis	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
23.	Noise Analysis	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
24.	Energy Analysis	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
25.	Asbestos	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
26.	Hazardous Waste	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
	Other Issues (list)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	

All supporting documentation can be located in the Environmental Appendix.

## NEPA ASSESSMENT CHECKLIST

Page 1 of 3

This checklist complies with FHWA regulations that implement NEPA, 23 CFR §771(1987), and was approved by the FHWA on July 15, 1996.

### I. GENERAL DEFINITION OF CATEGORICAL EXCLUSIONS

Before answering the questions on the NEPA Checklist, the preparer must be familiar with the general definition of Categorical Exclusions. Section 7.4 describes the general criteria for Categorical Exclusions according to 23 CFR 771. Sections 7.4.1.1 through 7.4 also define the Automatic Categorical Exclusions, Programmatic Categorical Exclusions and Categorical Exclusions with Documentation to which this NEPA Assessment Checklist applies.

## NEPA ASSESSMENT CHECKLIST

Answer the following questions by checking YES or NO.

### I. THRESHOLD QUESTION

- |  | YES   | NO  |
|--|-------|---|
| 1. Does the project involve unusual circumstances as described in 23 CFR §771.117(b)?  | _____ | _____ <input checked="" type="checkbox"/> |
| <ul style="list-style-type: none"><li>• If YES, the project does not qualify as a Categorical Exclusion and an EA or EIS is required. You may STOP COMPLETING THE CHECKLIST.</li><li>• If NO, go on.</li></ul> |       |   |

### II. AUTOMATIC CATEGORICAL EXCLUSION

- |   | YES                                       | NO    |
|---|---|-------|
| 2. Is the project an action listed as an Automatic Categorical Exclusion in 23 CFR §771.117(c) (C List) and/or is the project an element-specific project classified by FHWA as a Categorical Exclusion on July 22, 1996? | _____ <input checked="" type="checkbox"/> | _____ |
| <ul style="list-style-type: none"><li>• If YES to question 2, the project qualifies for a C List Categorical Exclusion. You may STOP COMPLETING THE CHECKLIST.</li></ul>  |   |       |

Note - Even if YES to question 2, there may be specific environmental issues that still require an action such as an EO 11990 Wetland Finding or a determination of effect on cultural resources. The project is still an Automatic Categorical Exclusion but the necessary action must be taken, such as obtaining FHWA's signature on the wetland finding. Refer to the appropriate section of the Environmental Procedures Manual for guidance.

- If NO to question 2, go on.

### III. PROGRAMMATIC CATEGORICAL EXCLUSION

- |  | YES   | NO    |
|--|-------|-------|
| 3. Is the project on a new location or involve a change in the functional classification or added mainline capacity (add through-traffic lanes)? | _____ | _____ |
| 4. Is this a Type I project under 23 CFR 772, <i>Procedures for Abatement of Highway Traffic Noise and Construction</i> ?                        | _____ | _____ |

- If YES to any question 3-20, project will not qualify as a Programmatic Categorical Exclusion. Answer questions 21 & 22 for documentation only and go on to question 23.

21. Does the project involve the use of a temporary road, detour or ramp closure? YES    NO  
\_\_\_\_\_

- If NO to questions 3-20 and NO to question 21, the project qualifies as a Programmatic Categorical Exclusion. You may STOP COMPLETING THE CHECKLIST. Refer to Section 8.6.2 of Chapter 8 of this manual for next steps.
- If YES to question 21, preparer should complete question 22 (i-v). If questions 3-20 are NO and 21 is YES, the project will still qualify as a Programmatic Categorical Exclusion if questions 22 (i-v) are YES.

22. Since the project involves the use of temporary road, detour or ramp closure, will all of the following conditions be met: YES    NO

i. Provisions will be made for pedestrian access, where warranted, and access by local traffic and so posted.	_____	_____
ii. Through-traffic dependent business will not be adversely affected.	_____	_____
iii. The detour or ramp closure, to the extent possible, will not interfere with any local special event or festival.	_____	_____
iv. The temporary road, detour or ramp closure does not substantially change the environmental consequences of the action.	_____	_____
v. There is no substantial controversy associated with the temporary road, detour or ramp closure.	_____	_____

- If questions 3-20 are NO, 21 is YES and 22 (i-v) are YES, the project qualifies for a Programmatic Categorical Exclusion. You may STOP COMPLETING THE CHECKLIST. Refer to Section 8.6.2 of Chapter 8 of this manual for next steps.
- If questions 3-20 are NO, 21 is YES and any part of 22 is NO, go on to question 23.

23. Is the project section listed in 23 CFR §771.117(d) (D List) or is the project an action similar to those listed in 23 CFR §771.117(d)? YES    NO  
\_\_\_\_\_

For those questions which precluded a Programmatic Categorical Exclusion, documentation should be provided for any YES response to questions 3-20 or for a NO response to any part of questions 22 (i-v). This documentation, as well as the checklist, should be included in the Design Approval Document, i.e., Final Design Report, to be submitted to the Regional Local Project Liaison for submission to the FHWA Division for classification of the project as a D List Categorical Exclusion. Refer to Chapter 8 of this manual for next steps.



**SEQR Type II Criteria Documentation (for minor highway projects per item 37 in 17 NYCRR 15.14(e))**

In accordance with 17 NYCRR 15.14(d) and 17 NYCRR 15.14(e)(37), this project is a SEQR Type II project. The project does not include or result in:

- (1) The acquisition of any occupied dwelling units or principal structures of business;
- (2) Significant changes in passenger or vehicle traffic volume, vehicle mix, local travel patterns or access (other than changes that would occur without the project);
- (3) more than minor social, economic or environmental effects upon occupied dwelling units, businesses, abutting properties or other established human activities;
- (4) Significant inconsistency with current plans or goals that have been adopted by local governmental bodies;
- (5) Physical alternation of more than 1.0 ha (2.5 acres) of publicly owned or operated parkland, recreation area or designated open space;
- (6) an effect on any historic district, site, building, structure or object that is listed, or may be eligible for listing, on the National Register of Historic Places, or any historic building, structure, site or prehistoric site that has been proposed by the Committee on the Registers for consideration by the New York State Board of Historic Preservation for a recommendation to the State Historic Preservation Officer for nomination for inclusion in said National Register;
- (7) more than minor alteration of, or adverse effect upon, any property, protected area, or natural or man-made resource of national, State or local significance, including but not limited to:
  - (i) Freshwater or tidal wetlands and associated areas;
  - (ii) Floodplain areas;
  - (iii) Prime or unique agricultural land;
  - (iv) Agricultural districts so designated pursuant to article 25, section 203, when more than one acre of such district may be affected;
  - (v) Water resources, including lakes, reservoirs, rivers, streams;
  - (vi) Water supply sources;
  - (vii) Designated wild, scenic and recreational rivers;
  - (viii) Unique ecological, natural wooded or scenic areas;
  - (ix) Rare, endangered or threatened species formally designated as such pursuant to Federal law; and
  - (x) Any area officially designated as a critical environmental area pursuant to 6 NYCRR Part 617; and
- (8) The requirement for an indirect air source quality permit, pursuant to 6 NYCRR Part 203.

# Driving Park Bridge



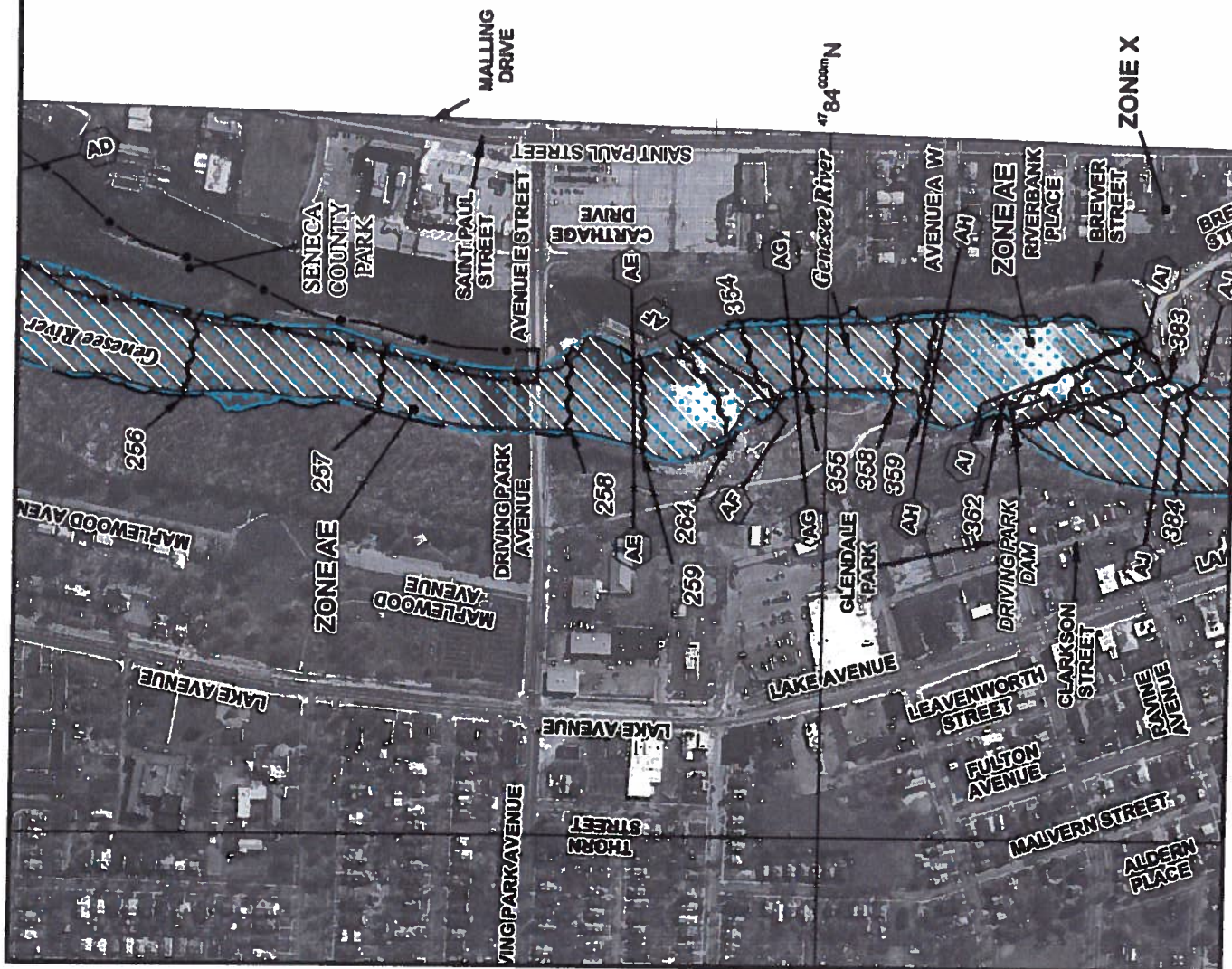
December 16, 2011

Disclaimer: This map was prepared by the New York State Parks, Recreation and Historic Preservation National Register Listing Internet Application. The information was compiled using the most current data available. It is deemed accurate, but is not guaranteed.





MAP SCALE 1" = 500'



<b>NFIP</b>	
<b>NATIONAL FLOOD INSURANCE PROGRAM</b>	
<b>PANEL 0192G</b>	<b>FIRM</b>
<b>FLOOD INSURANCE RATE MAP</b>	
for MONROE COUNTY, NEW YORK (ALL JURISDICTIONS)	
<b>CONTAINS:</b>	<b>NUMBER</b>
<b>COMMUNITY</b>	<b>360431</b>
<b>ROCHESTER, CITY OF</b>	
<b>PANEL 192 OF 528</b>	
<b>MAP SUFFIX: G</b>	
(SEE MAP INDEX FOR FIRM PANEL LAYOUT)	
Notice to User: The Map Number shown below should be used when placing map orders: the Community Number shown above should be used on insurance applications for the mapped community.	
	<b>MAP NUMBER</b> <b>36055C0192G</b>
<b>EFFECTIVE DATE</b> <b>AUGUST 28, 2008</b>	
Federal Emergency Management Agency	

This is an official copy of a portion of the above referenced flood map. It was extracted using F-MIT On-Line. This map does not reflect changes or amendments which may have been made subsequent to the date on the title block. For the latest product information about National Flood Insurance Program flood maps check the FEMA Flood Map Store at [www.fema.gov](http://www.fema.gov)





Search	Layers & Legend	Tell Me More...
Need a Permit?	Contacts	Help

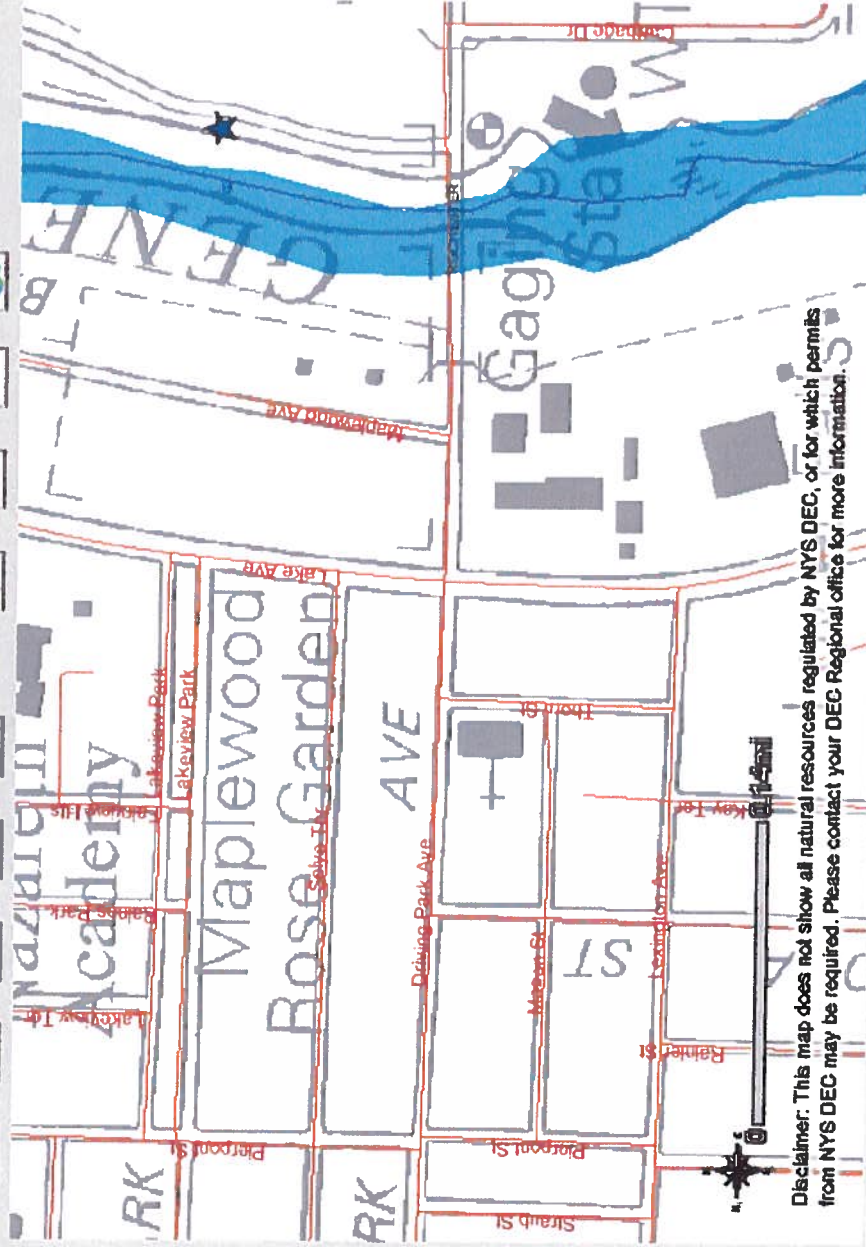
### Map Layers & Legend

More layers appear as you zoom in.

- ☒ Classified Water Bodies
- ☒ Unique Geological Features
- ☒ Classified Water Bodies
- ☒ State-Regulated Freshwater Wetlands
- ☒ Wetland Checkzone ?
- ☒ Rare Plants and Rare Animals
- ☒ Significant Natural Communities
- ☒ Natural Communities Vicinity ?
- ☒ Background Map
- ☒ Adirondack Park Boundary
- ☒ Counties

Click "Refresh Layers" to activate and deactivate layers.

[Refresh Layers](#)



Disclaimer: This map does not show all natural resources regulated by NYS DEC, or for which permits from NYS DEC may be required. Please contact your DEC Regional office for more information.



U.S. Fish and Wildlife Service

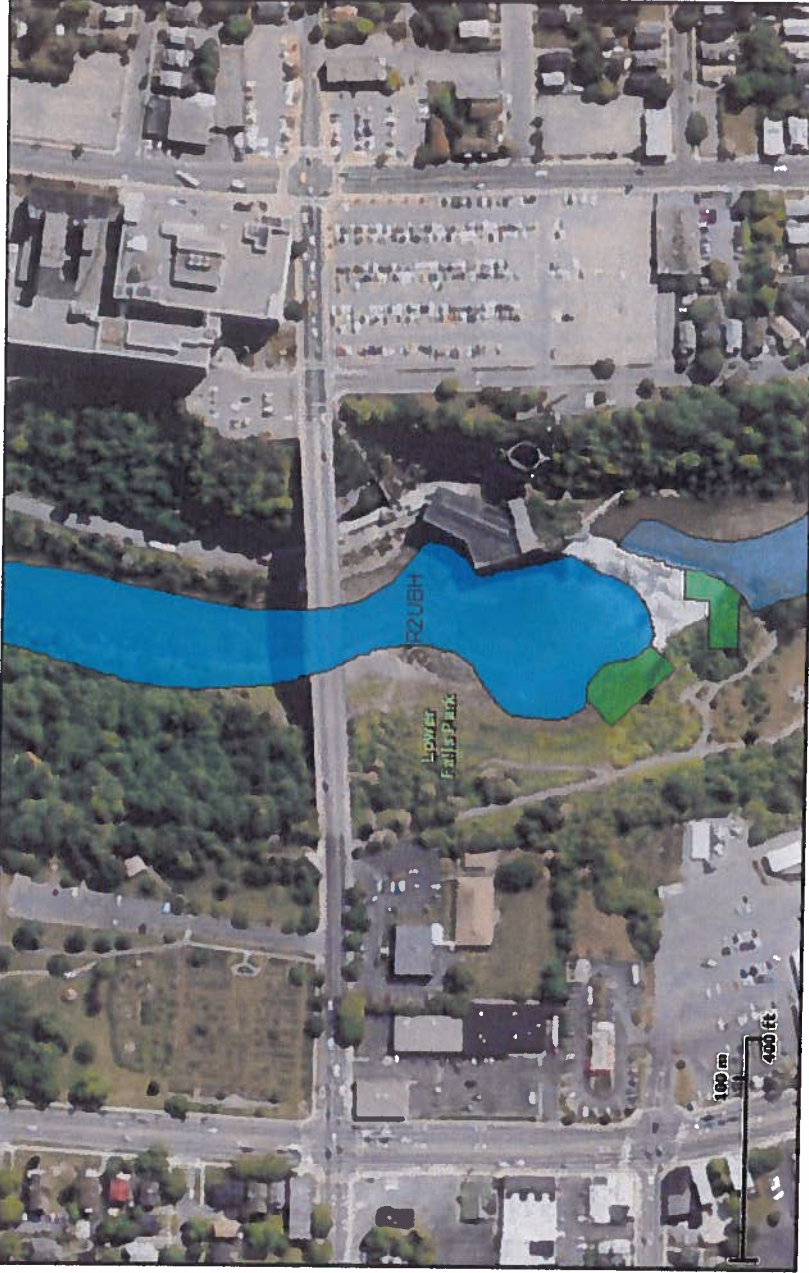
## National Wetlands Inventory

Driving Park  
Bridge

Dec 16, 2011

### Wetlands

- Freshwater Emergent
- Freshwater Forested/Shrub
- Estuarine and Marine Deepwater
- Estuarine and Marine
- Freshwater Pond
- Lake
- Riverine
- Other



This map is for general reference only. The US Fish and Wildlife Service is not responsible for the accuracy or completeness of the information shown on this map. All wetlands related data should be used in accordance with the paper publications found on the Wetlands Mapper web site.

User Remarks:



[\[print page\]](#)[\[close window\]](#)**The Coordinates of the point you clicked on are:**

NYTM	E : 286405	Longitude/Latitude	W : 77.628
	N : 4784391		N : 43.182

**Classified Streams**

Regulation	Standard	Classification
820-1	B	B

**Old or Potential Records (these records are not displayed on the map)**

Common Name	Scientific Name	Date Last Documented	Location	Habitat Where Last Seen	Animal, Plant, or other	NYS Protected Status
Purple Bluets	Houstonia purpurea var. purpurea	1905-09-03	Genesee Falls	River banks.	Rare Plant	Endangered
American Burying Beetle	Nicrophorus americanus	no date	Rochester		Rare Animal	Endangered

**USGS Quadrangle**

USGS Quadrangle Name
ROCHESTER WEST

If your project or action is within or near an area with a rare animal, a permit may be required if the species is listed as endangered or threatened and the department determines the action may be harmful to the species or its habitat.

If your project or action is within or near an area with rare plants and/or significant natural communities, the environmental impacts may need to be addressed.

The presence of a unique geological feature or landform near a project, unto itself, does not trigger a requirement for a NYS DEC permit. Readers are advised, however, that there is the chance that a unique feature may also show in another data layer (ie. a wetland) and thus be subject to permit jurisdiction.

Please refer to the "Need a Permit?" tab for permit information or other authorizations regarding these natural resources.

**Disclaimer:** If you are considering a project or action in, or near, a wetland or a stream, a NYS DEC permit may be required. The Environmental Resources Mapper does not show all natural resources which are regulated by NYS DEC, and for which permits from NYS DEC are required. For example, Regulated Tidal Wetlands, and Wild, Scenic, and Recreational Rivers, are currently not included on the maps.



**Monroe County****Federally Listed Endangered and Threatened Species and Candidate Species**

This list represents the best available information regarding known or likely County occurrences of Federally-listed and candidate species and is subject to change as new information becomes available.


<u>Common Name</u>	<u>Scientific Name</u>	<u>Status</u>
Bog turtle (Riga and Sweden Townships)	<i>Clemmys [=Glyptemys] muhlenbergii</i>	T

Status Codes: E=Endangered, T=Threatened, P=Proposed, C=Candidate, D=Delisted.

Information current as of: 12/16/2011

New York State State Agencies

Search all of NY.gov

DEPARTMENT OF ENVIRONMENTAL CONSERVATION

Printer-friendly || A-Z Subject Index || Enter search words Search DEC

Home » Animals, Plants, Aquatic Life » Insects & Other Species » American Burying Beetle Fact Sheet

## Outdoor Activities

## Animals, Plants, Aquatic Life

## Insects &amp; Other Species

## American Burying Beetle Fact Sheet

## Chemical &amp; Pollution Control

## Energy and Climate

## Lands and Waters

## Education

## Permits and Licenses

## Public Involvement and News

## Regulations and Enforcement

## Publications, Forms, Maps

## About DEC

## American Burying Beetle Fact Sheet

### American Burying Beetle *Nicrophorus americanus*

New York Status: **Extirpated**Federal Status: **Endangered**

### Description

The American burying beetle, also known as the "giant carrion beetle," is the largest member of its genus in North America. Most adults are 1.2 inches (30 mm) in length, though they vary from 1.0 -1.4 inches (25-35mm). This beetle can be easily identified by its distinctive orange-red on shiny black coloration.



One colored mark covers the frons, an upper frontal head plate, and a similarly colored plate exists just behind the head. Both contrast sharply with the black body color. Wings are black with two pairs of scalloped red spots and the tips on the antennae are orange. The sexes can be distinguished by a distinctively shaped orange-red facial mark below the frons. Males have a large rectangular mark, while females have a smaller triangular mark.

Burying beetles often carry swarms of orange-colored mites on their body. They help keep beetles and carcasses clean of microbes and fly eggs.

### Life History

American burying beetles are active from late April through September. Adults are nocturnal, active when temperatures exceed 15C (60F). Most reproductive activity and carcass burial occur in June and July. Reproduction depends on the availability of carrion. American burying beetles select carcasses larger than other burying beetles. The carcasses of larger species (i.e. pheasant chicks) are used as a food source during the breeding

### Important Links

[Endangered Species Program](#)

### Contact for this Page

Endangered Species Unit  
NYSDEC  
625 Broadway  
Albany, NY 12233-4754  
518-402-8924  
[Send us an email](#)

### This Page Covers



All of  
New York State

layer are characteristic of all sites. Open agricultural land is frequently utilized. It is unlikely that vegetational structure and soil type were historically limiting, in a general sense, considering the species' wide geographic range. While soils suitable for carcass burial are essential, it is probably carrion availability that is more important. Vegetation and soil do influence the potential prey base available to the beetles, though. Historically, American burying beetles depended upon large aggregations of 100-200 gram carcasses; ring-necked pheasant chicks were ideally suited. Today on Block Island, large 100-200 gram carcasses are used from six bird species, including pheasants and woodcock. Twice as abundant, small carcasses (<100 g) are also utilized.

## Status

In addition to the known populations in Rhode Island and Oklahoma, American burying beetles were collected in Ontario, Kentucky, Arkansas, Missouri and Nebraska as late as 1970. If the species still exists in these areas, it is very localized.

The decline of American burying beetles has been underway for almost a century. Populations were largely gone by the 1920's. The prevailing theory for the decline involves habitat loss and fragmentation, which led to a greatly reduced carrion food-base. With habitat fragmentation, high population densities of many indigenous species were no longer possible. Species composition possibly changed. Changing land use patterns resulted in increased acreage of agricultural land; species composition in these habitats also changed. Mice were more plentiful, but at 25 grams were too small for the beetles. Passenger pigeons and prairie chickens disappeared. Turkey, waterfowl and shorebird populations declined. Prey species were generally less plentiful. Widespread cutting of forests increased edge habitat, which led to more predators and scavengers such as foxes, raccoons, opossums, skunks and crows. All competed with the beetles for carrion. The optimum-sized, carrion food-base was reduced throughout the beetle's range. The beetle disappeared.

Other theories for the decline exist. DDT was unlikely responsible, for the decline had occurred 25 years before DDT was used. A species specific disease is unlikely, though not impossible. Populations of other carrion beetle species have remained largely intact. American burying beetles appear to have broad habitat tolerances, so direct habitat loss was unlikely responsible initially. Once populations of burying beetles become isolated, though, habitat loss can become an important factor. Movements between habitats occurs less frequently.



**Purple Bluets**  
*Houstonia purpurea*  
Rubiaceae (Madder or Coffee) Family



**Plant is an upright (sometimes reclining), many-stemmed perennial with a rhizome from which new growth emerges; 4-16 inches tall. Preferred habitat is rich woods, at roadsides and flood plains. Distribution is throughout the Escambia region.**

**Leaves are opposite, sessile (no leaf stalks), egg-shaped and usually no more than 1-1/2 inches long; no teeth and no lobes.**

**Flowers are white to pale purple with funnel-shaped corolla; downy hair inside the throat; small; short leaf stalks. Flowers occur in the spring.**

**Fruit is a capsule.**



**Appendix 7-9**

**Project Submittal Package – Section 106 of the National Historic Preservation Act**

NEW YORK STATE DEPARTMENT OF TRANSPORTATION PROJECT SUBMITTAL PACKAGE  
**Section 106 of the National Historic Preservation Act**  
**For Locally-Administered Federal-Aid Projects**

A Project Submittal Package is prepared by the Local Project Sponsor (Sponsor) or their consultants for federal aid transportation projects to provide sufficient information for NYSDOT assessment of Section 106 obligations. The Sponsor sends the package to the Regional Local Project Liaison (RLPL) for RCRC review. The RCRC will make recommendations to identify what is needed for Section 106 compliance for the project.

DATE 2/10/12 PIN 4755.30 BIN 2211310

**IDENTIFICATION**

Project Name (if any) DRIVING PARK AVENUE BRIDGE OVER GENESEE RIVER  
Project Area Boundaries LIMITS OF BRIDGE STRUCTURE ONLY

(Indicate State or County Route # and/or local street name, and clearly defined endpoints)

County MONROE Town/City ROCHESTER Village/Hamlet: \_\_\_\_\_

Have you consulted the NYSHPO web site at [\\*http://nysparks.state.ny.us](http://nysparks.state.ny.us) to determine the preliminary presence or absence of previously identified cultural resources within or adjacent to the project area? If yes:

☒ Yes ☐ No

- Was the project site wholly or partially included within an identified archaeologically sensitive area?
- Does the project site involve or is it substantially contiguous to a previously evaluated National Register of Historic Places listed property?

☒ Yes ☐ No

☐ Yes ☒ No

[\\*http://nysparks.state.ny.us](http://nysparks.state.ny.us) then select HISTORIC PRESERVATION then Historic Preservation Field Services Bureau then On Line Tools

**ALL PROJECTS SUBMITTED FOR REVIEW SHOULD INCLUDE THE FOLLOWING INFORMATION**

- ☒ **Project Description** – Attach a full description of the nature and extent of the work to be undertaken as part of this project. This should include, but not limited to, potential activities that might involve drainage, cutting, excavation, grading, filling, on-site detours, new sidewalks, right-of-way acquisition. Relevant portions of the project applications or environmental statements may be submitted. This could be from sections of the Draft Design Report/ Draft Scoping Document.
- ☒ **Location Maps** - Provide USGS Quad or DOT Planimetric map showing project area location. The map must clearly show street and road names surrounding the project area as well as all portions of the project.
- ☒ **Photos** - Provide clear, original color photographs of the entire project area keyed to a site plan. These photos should indicate:
  - Buildings/structures more than 50 years old that are located along the property or on adjoining property
  - Areas of prior ground disturbance (removal of original topsoil; filling and plowing are not considered disturbance)

**LOCAL SPONSOR CONTACT**

Name TOM HACK, P.E. Title CITY BRIDGE ENGINEER  
Firm/Agency CITY OF ROCHESTER DEPARTMENT OF ENVIRONMENTAL SERVICES  
Address 30 CHURCH STREET, RM 300B City ROCHESTER State NY Zip 14614  
Phone 585.428.6852 E-Mail TOM.HACK@CITYOFROCHESTER.GOV  
Consultant Name & Phone LABOLA ASSOCIATES, P.C. (585) 295.6287  
ATTN: JOHN PAPPONETTI, P.E.

Driving Park Avenue Bridge over Genesee River  
BIN 2211310  
PIN 4755.30

**PROJECT DESCRIPTION:**

The Driving Park Avenue Bridge over Genesee River project involves the following element specific preventative maintenance tasks:

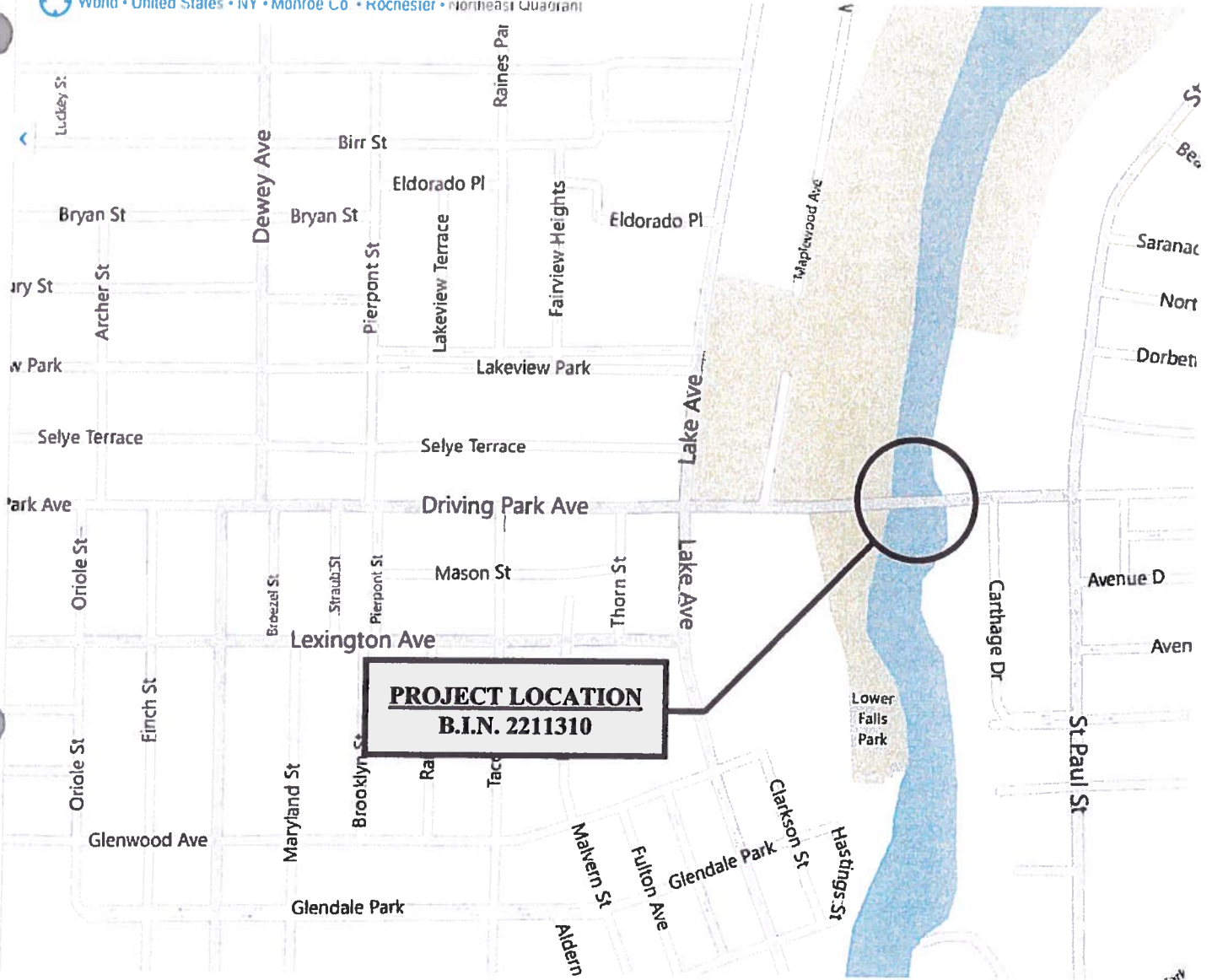
- Remove & Replace Span 3 Structural Deck
- Remove & Replace Span 3 Sidewalks
- Remove/Store & Reset Span 3 Bridge Railing & Fencing
- Remove & Reset Bridge Curb
- Replace Bridge Curb
- Replace Expansion Joints
- Paint Girder Ends & Floorbeams at Joint Locations
- Clean and Paint Bearings under Joints
- Remove and Reset Light Poles on Span 3
- Repair & Flush Scuppers
- Wash Bridge

The project is located within an Archeological Sensitive Area, however given the nature of the element specific preventative maintenance tasks, it is anticipated that the only impact will be to the existing bridge superstructure.

The Bog Turtle is a threatened species known to be found in the Town of Riga (outside of project limits). The American Burying Beetle is an endangered animal known to be found in the Rochester area at one time. Based on available NYSDEC documents, the American Beetle is known to exist in only two locations, Block Island, RI and Eastern Oklahoma. NYSDEC also indicates that the Purple Bluets is known to exist along the river banks of the Genesee River. This project does not propose any activities that would impose a negative impact on endangered or threatened species.

It is assumed that the project will be progressed as a SEQR Type II and NEPA Class II Automatic Categorical Exclusion.





## PROJECT LOCATION MAP

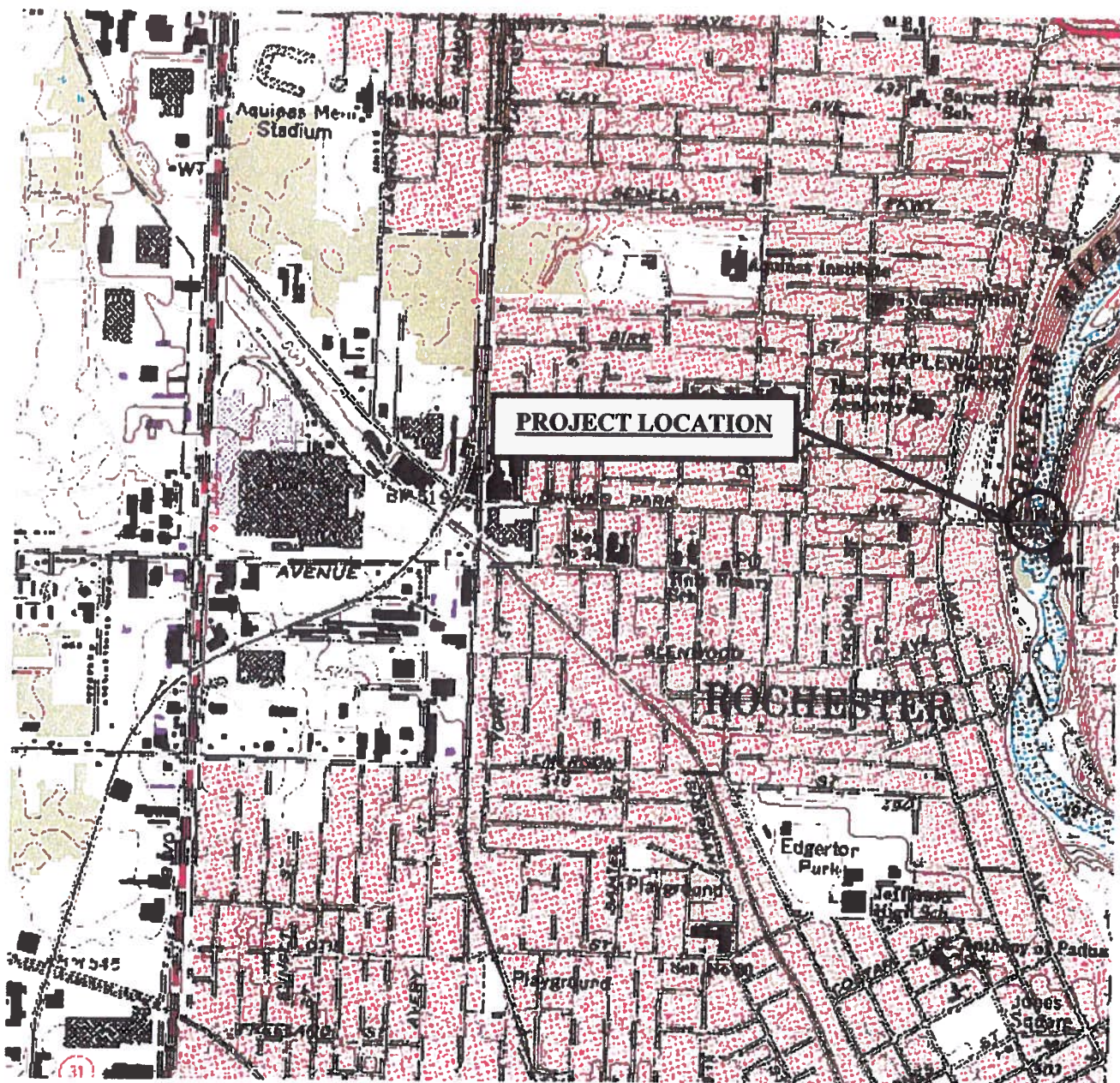
**Driving Park Avenue Bridge  
over Genesee River  
(BIN 2211310)**

**City of Rochester  
Monroe County, New York**

**ABELLA**  
Associates, P.C.

PROJECT NO.: 207650.04





## USGS LOCATION MAP

Driving Park Avenue over Genesee River  
(BIN 2211310)

USGS Quadrangle Map: Rochester West

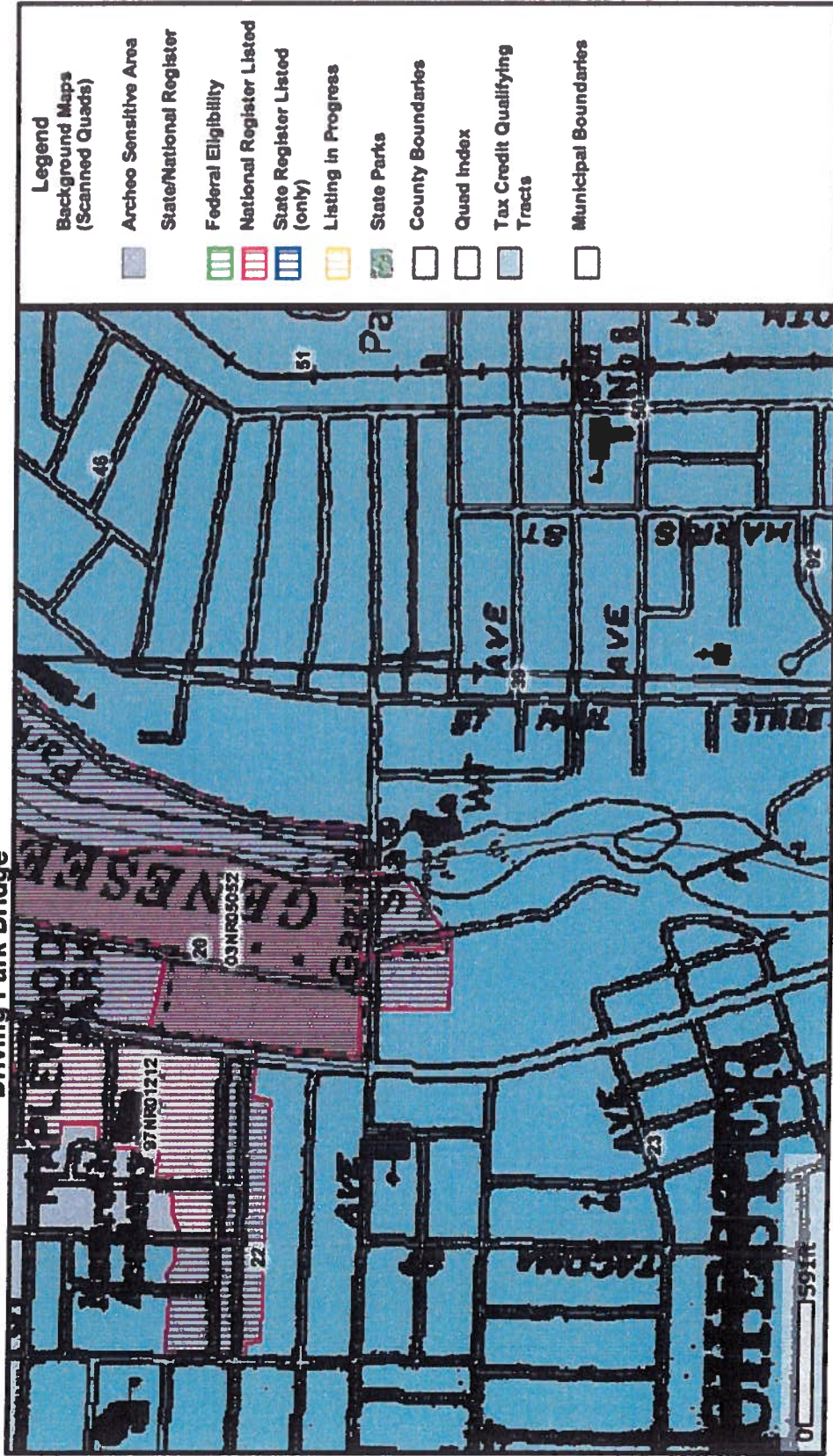
City of Rochester  
Monroe County, New York

**ABELLA**  
Associates, P.C.

PROJECT NO.: 207650.04



### Driving Park Bridge



December 16, 2011

Disclaimer: This map was prepared by the New York State Parks, Recreation and Historic Preservation National Register Listing Internet Application. The information was compiled using the most current data available. It is deemed accurate, but is not guaranteed.



Driving Park Avenue Bridge over Genesee River  
City of Rochester, New York  
February 10, 2012



1. Elevation looking North



2. West Approach looking East

Driving Park Avenue Bridge over Genesee River  
City of Rochester, New York  
February 10, 2012



3. East Approach looking West



4. Looking North (Downstream)



Driving Park Avenue Bridge over Genesee River  
City of Rochester, New York  
February 10, 2012



5. Looking South (Upstream)



6. South Fascia





MEMORANDUM  
DEPARTMENT OF TRANSPORTATION

**TO:** Frank DiCostanzo, Region Local Project Liaison  
**FROM:** Chris Caraccilo, Regional Cultural Resource Coordinator  
**SUBJECT:** PROJECT SUBMITTAL PACKAGE – SECTION 106 RECOMMENDATIONS  
PIN 4755.30, Driving Park Avenue Bridge over the Genesee River-BIN 221310,  
City of Rochester, Monroe County  
**DATE:** February 14, 2012

As the Regional Cultural Resource Coordinator (RCRC) I have reviewed the Project Submittal Package (PSP) prepared for the above referenced Locally-Administered Federal-Aid project for assessment of obligations under Section 106 of the National Historic Preservation Act (36 CFR Part 800).

Based on review of this PSP, I conclude:

- ☒ The project activities have no potential to cause effects on historic properties in accordance with 36 CFR 800.3(a)(1) therefore, there are no further obligations for compliance with Section 106 of the National Historic Preservation Act. This determination should be recorded in the project environmental documentation.
- ☐ The project activities may cause effects on historic properties. A Cultural Resource Survey is needed to identify historic and cultural resources.
- ☐ A Finding Documentation package is needed to assess the project effect on (a previously National Register (NR) listed property) \_\_\_\_\_.
- ☐ The following additional information is needed to complete our assessment:
- ☐ Detailed project description
  - ☐ Project location map showing project limits (USGS Quad)
  - ☐ Photos of prior ground disturbance
  - ☐ Photos of buildings
  - ☐ Information from SHPO web site (archaeological sensitivity and NR listed buildings)
  - ☐ Other

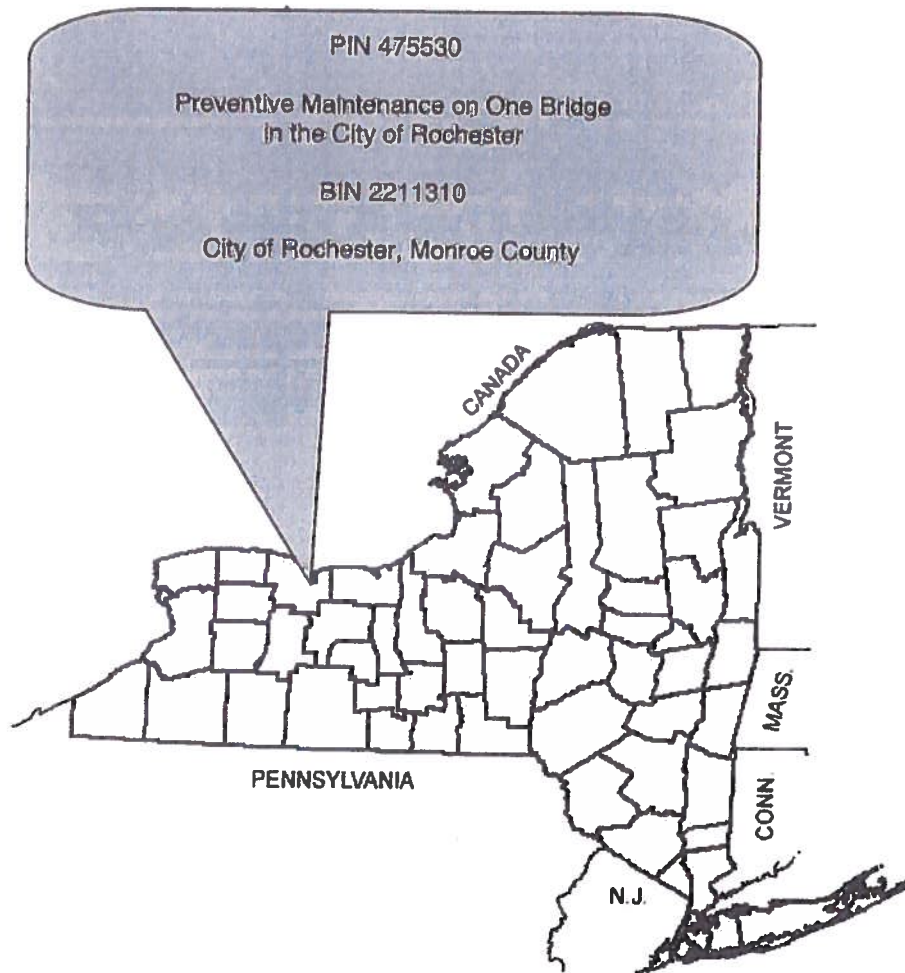
# **APPENDIX F**

**Original IPP**

# TRANSPORTATION

## INITIAL PROJECT PROPOSAL

February 2011



PROJECT REPORT

U.S. Department of Transportation Federal Highway Administration

NEW YORK STATE DEPARTMENT OF TRANSPORTATION  
Andrew Cuomo, Governor

Joan McDonald, Commissioner





**PROJECT APPROVAL SHEET**

(Pursuant to SAFETEA-LU Matrix)

**Milestones****Signatures****Dates****A. Recommendation for  
IPP Approval:**

The project cost and schedule are consistent with the Regional Capital Program.

  
Regional Program Manager3/29/11**B. IPP Approval:**The project is ready to be added to the Regional Capital Program  
and project scoping can begin.  
Regional Director3/29/11

**PIN:** 475530**PROJECT NAME:** Preventive Maintenance for One Bridges in the City of Rochester**MUNICIPALITIES:** City of Rochester**COUNTY:** Monroe**ROUTE/STATE HIGHWAY NUMBER:** NA**BIN:** 2211310, Driving Park Avenue over the Genesee River**LIMITS:** Milepoints: NA  
Reference Markers: NA**PROJECT LENGTH:** NA**FEDERAL AID SYSTEM:** FA, non-NHS**FUNCTIONAL CLASS:** urban minor arterial

**EXISTING CHARACTERISTICS OF CONCERN:** Due to normal wear, this bridge is in need of certain corrective maintenance work to span 3 in order to continue to function as designed. There are no critical characteristics of concern but chronic tranverse cracking is causing inordinate leakage over the span 3 floorbeams.

**ELEMENT****MEASURE/INDICATOR**

BIN 2211310

Condition Rating is 4.903; Sufficiency Rating is 94.7

- Structural Deck at span 3 is rated '4'
- Expansion Joints at span 3 are rated '3'
- Paint at span 3 is rated '2'

**PROJECT OBJECTIVE:** This project would improve this bridge in order to extend its effective service life by 15 to 20 years.

**PROJECT ELEMENTS TO BE INVESTIGATED:**

- |  |   |
|--|---|
| <input checked="" type="checkbox"/> Deck/Minor Bridge Rehabilitation | <input type="checkbox"/> Bridge Replacement, New Location         |
| <input type="checkbox"/> Major Bridge Rehabilitation                 | <input type="checkbox"/> Bridge Replacement, Existing Location    |
| <input type="checkbox"/> Highway Resurface                           | <input type="checkbox"/> Highway Reconstruction                   |
| <input type="checkbox"/> Appurtenance                                | <input type="checkbox"/> Large Culvert Rehabilitation/Replacement |
| <input type="checkbox"/> Traffic Control                             | <input type="checkbox"/> Other:                                   |

**PROPOSED WORK DESCRIPTION:** The work to be undertaken would include joint replacement, partial deck replacement, and isolated painting.

**PRIORITY RESULTS:** ☒ Mobility & Reliability ☐ Safety ☐ Security  
☐ Economic Competitiveness ☐ Environmental Stewardship

**FUNDING SOURCE:** ☐ 100% State ☒ Federal (HBP)

**RECOMMENDED ENVIRONMENTAL CLASSIFICATION:**

PROJECTED ENVIRONMENTAL PROCESS:				
NEPA:	<input type="checkbox"/> No Federal Funds	<input checked="" type="checkbox"/> Class II, CE <input type="checkbox"/> CE/Auto <input checked="" type="checkbox"/> CE/Prog <input type="checkbox"/> CE/Doc	<input type="checkbox"/> Class III, EA <input type="checkbox"/> SAFTEA-LU Applies	<input type="checkbox"/> Class I, EIS <input type="checkbox"/> SAFTEA-LU Applies
SEQR:	<input type="checkbox"/> Exempt	<input checked="" type="checkbox"/> Type II	<input type="checkbox"/> Non-Type II <input type="checkbox"/> EA -or-	<input type="checkbox"/> EIS

The following Checklist will be prepared during scoping/preliminary engineering:

- ☒ NEPA Checklist  
☐ Regional Environmental Checklist  
☐ Landscape Architectural/ Environmental Services IPP Report

**MPO INVOLVEMENT:** ☐ No ☒ Yes, TIP Name: PM on 1 Bridge in the City of Rochester  
TIP Number: B11-19-MN1

**TIP AMENDMENT REQUIRED:** ☒ No ☐ Yes, Needed by:

**STIP STATUS:** ☒ On STIP ☐ Not on STIP

**MOU STATUS:** The PIN is not in the 2010/2011 MOU.

**NOTES ON SPECIAL CIRCUMSTANCES:** Scoping, design and construction are to be administered by the City Structural Engineering Office. The sponsor's project manager is Tom Hack, Senior Structures Engineer (585.428.6852).

**SPECIAL TECHNICAL ACTIVITIES REQUIRED:** A State-Local agreement will be required to allow for reimbursement of sponsor expenditures consistent with the applicable Federal Aid Program.

**PLANNED PUBLIC INVOLVEMENT:** A Public Involvement Plan indicating how the public will be made aware of the construction activities will be prepared during preliminary engineering.

**WORKZONE SAFETY & MOBILITY:** The Region has determined that the subject project is not significant per 23 CFR 630.1010. A Transportation Management Plan consisting of a temporary work zone traffic control plan will be prepared during preliminary engineering. Coordination with the Regional Transportation Operations Center and public information activities will be considered during final design.

**PROBABLE SCHEDULE AND COST:** Scoping (SLA execution and consultant acquisition) would begin in October 2012. Preliminary engineering would begin in February 2013; final design would begin in June 2013. The PS&E would be produced in October 2013 for a bid opening in December 2013. Contract award and construction start would be in February 2014. The estimated cost of design, construction, inspection, and administration is \$3,200,000.

**DESIRED LETTING:** 12/6/13

**DESIRED CONSTRUCTION COMPLETION:** 8/30/14

**SCHEDULE QUALIFIERS:**

<input type="checkbox"/> Public Hearing	<input type="checkbox"/> 4(f)/106
<input type="checkbox"/> Major Permits	<input type="checkbox"/> Real Estate
<input checked="" type="checkbox"/> Consultant	<input checked="" type="checkbox"/> Other: SLA



**PROGRAMMING:**

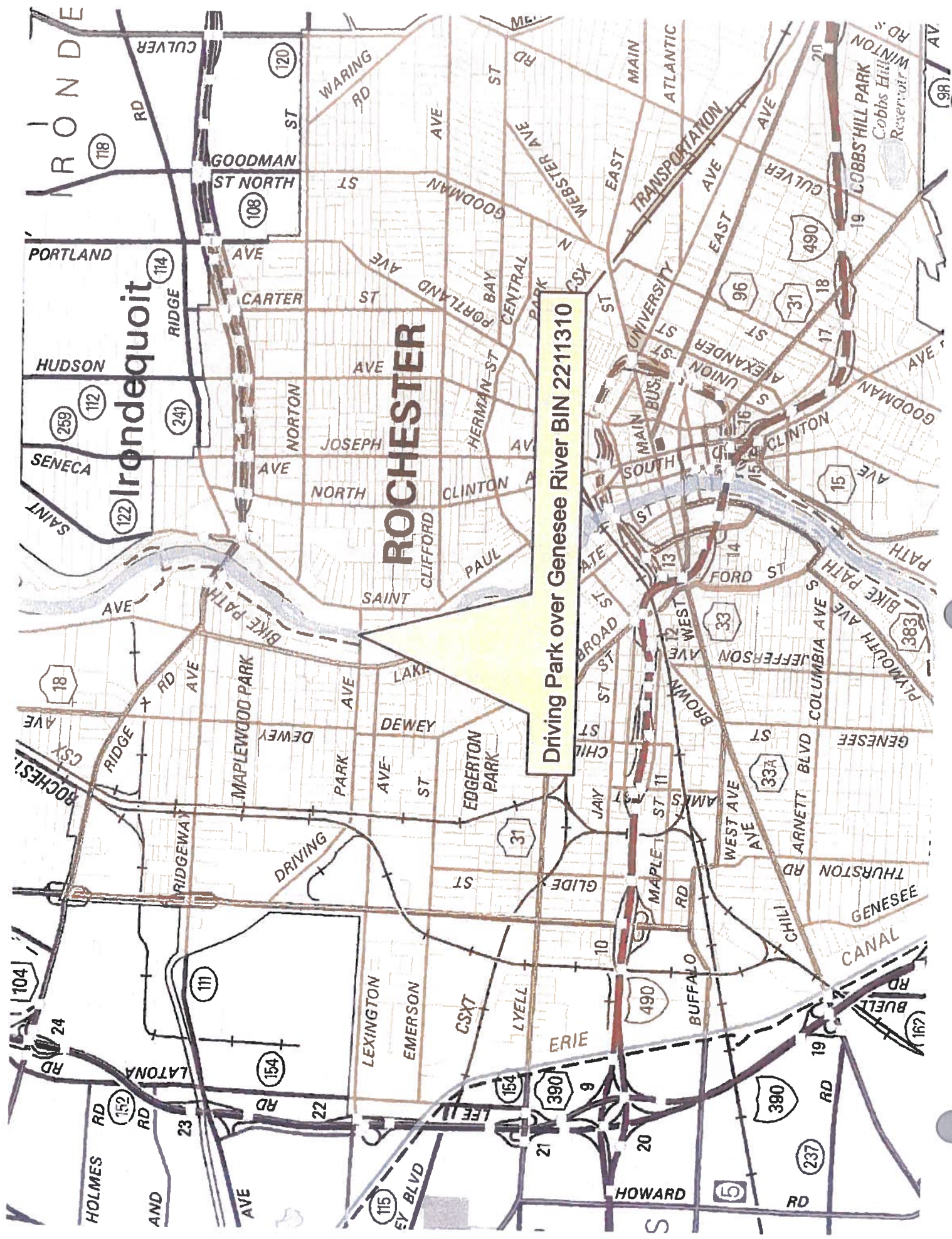
Project Phase	Activity Duration	Estimated Cost (\$m)	Fund Source	Obligation Date
Scoping	4 months	0.050	local	10/7/12
Preliminary Engineering	4 months	0.150	local	2/3/13
Final Design	4 months*	0.250	FA (HBP)	6/1/13
Construction	7 months**	2.500	FA (HBP)	10/6/13
Construction Inspection	7 months**	0.250	FA (HBP)	10/6/13
<b>TOTAL</b>		<b>3.200</b>		

\* to PS&amp;E (10/6/13) \*\* from award (2/6/14)

**BASIS OF ESTIMATE:** Sponsor's scoping report for its TIP application**PROJECT CATEGORY:** ☐ Simple ☒ Moderate ☐ Complex**STATEWIDE SIGNIFICANCE:** ☒ No ☐ Yes  
Remarks:**ASSET MANAGEMENT:**

AM Team	IPP Initiator	Asset Specific Cost Share	Asset Team Specific Cost/Scope/Schedule/Concurrence
Local Projects	RPPM	\$2,500,000	Rick Papaj

**ASSIGNED PROJECT MANAGER:** Rick Papaj**PHONE:** 585 272 3466**FUNCTIONAL AREA:** Regional Planning and Program Management**IPP PREPARED BY:** Edwin Welsh**DATE:** 2/23/11



Driving Park over Genesee River BIN 2211310